



**Notice of a public meeting of  
Planning Committee A**

- To:** Councillors Cullwick (Chair), Pavlovic (Vice-Chair), Ayre, Barker, D'Agorne, Doughty, Kilbane, Fisher, Looker, Melly and Waudby
- Date:** Thursday, 4 August 2022
- Time:** 4.30 pm
- Venue:** The George Hudson Board Room - 1st Floor West Offices (F045)

**AGENDA**

- 1. Declarations of Interest**  
At this point in the meeting, Members are asked to declare any disclosable pecuniary interest or other registerable interest they might have in respect of business on this agenda, if they have not already done so in advance on the Register of Interests.
- 2. Minutes** (Pages 1 - 18)  
To approve and sign the minutes of the meetings of the Planning Committee A held on 7 July and 11 July 2022.
- 3. Public Participation**  
At this point in the meeting members of the public who have registered to speak can do so. Members of the public may speak on agenda items or on matters within the remit of the committee.

**Please note that our registration deadlines have changed to 2 working days before the meeting, in order to facilitate the management of public participation at meetings. The deadline for registering at this meeting is 5:00pm on Tuesday 2 August 2022.**

To register to speak please visit [www.york.gov.uk/AttendCouncilMeetings](http://www.york.gov.uk/AttendCouncilMeetings) to fill in an online registration form. If you have any questions about the registration form or the meeting, please contact Democratic Services. Contact details can be found at the foot of this agenda.

## **Webcasting of Public Meetings**

Please note that, subject to available resources, this meeting will be webcast, including any registered public speakers who have given their permission. The meeting can be viewed live and on demand at [www.york.gov.uk/webcasts](http://www.york.gov.uk/webcasts).

### **4. Plans List**

This item invites Members to determine the following planning applications:

- a) **Railway Museum, Leeman Road, York** (Pages 19 - 148)  
**[21/02793/REMM]**  
Reserved matters application for layout, scale, appearance, landscaping and access for the construction of Central Hall (F1 use class) including entrance hall, exhibition space and café with associated access, parking, landscaping and external works following the demolition of the mess room and other structures pursuant to 18/01884/OUTM [Holgate Ward]
- b) **Oak Haven, 144 Acomb Road, York, YO24 4HA** (Pages 149 - 184)  
**[22/00304/FULM]**  
Erection of 64 bedroom residential care home (use class C2) with associated structures, access, parking and landscaping following demolition of existing structures [Holgate Ward]
- c) **York Wheelchair Centre, Bluebeck House,** (Pages 185 - 232)  
**Bluebeck Drive, York YO30 5RA**  
**[22/00707/FULM]**  
Erection of 72no. bedroom care home (use class C2) with associated landscaping following demolition of Blue Beck House and outbuildings [Rawcliffe And Clifton Without Ward]

### **5. Urgent Business**

Any other business which the Chair considers urgent under the Local Government Act 1972.

Democracy Officer

Angela Bielby

Contact details:

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For more information about any of the following please contact the Democratic Services Officer responsible for servicing this meeting:

- Registering to speak
- Business of the meeting
- Any special arrangements
- Copies of reports and
- For receiving reports in other formats

Contact details are set out above.

**This information can be provided in your own language.**

我們也用您們的語言提供這個信息 (Cantonese)

এই তথ্য আপনার নিজের ভাষায় দেয়া যেতে পারে। (Bengali)

Ta informacja może być dostarczona w twoim własnym języku. (Polish)

Bu bilgiyi kendi dilinizde almanız mümkündür. (Turkish)

یہ معلومات آپ کی اپنی زبان (بولی) میں بھی مہیا کی جاسکتی ہیں۔ (Urdu)

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City of York Council

Committee Minutes

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Meeting	Planning Committee A
Date	7 July 2022
Present	Councillors Cullwick (Chair), Pavlovic (Vice-Chair), Ayre, Barker, Fisher, Looker, Rowley (Substitute for Cllr Doughty), Crawshaw (Substitute for Cllr Melly), Baker (Substitute for Cllr D'Agorne), Lomas (Substitute for Cllr Kilbane) and Orrell (Substitute for Cllr Waudby)
In Attendance	Becky Eades (Head of Planning and Development Services) Louise Milnes (Development Management Officer) Heidi Lehane (Senior Solicitor) Helene Vergereau (Traffic and Highway Development Manager) Tony Clarke (York Central Highway Authority Lead)
Apologies	Councillors D'Agorne, Melly, Kilbane, Doughty, Waudby

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## **6. Declarations of Interest 16:36**

Members were asked to declare, at this point in the meeting, any personal interests, not included on the Register of Interests, or any prejudicial or disclosable pecuniary interests they may have in respect of business on the agenda. None were declared.

## **7. Public Participation 16:37**

It was reported that there had been no registrations to speak at the meeting under the Council's Public Participation Scheme on general matters within the remit of the Planning Committee.

## **8. Minutes 16:37**

Resolved: That the minutes of the last meeting of Planning Committee A held on 9 June 2022 be approved and then signed by the chair as a correct record.

**9. Plans List**  
**16:37**

Members considered a schedule of reports of the Head of Planning and Development Services, relating to the following planning applications, outlining the proposals and relevant policy considerations and setting out the views of consultees and officers.

**2a) Railway Museum, Leeman Road, York [21/02793/REMM] 16:37**

Members considered a major Reserved matters application from the Board of Trustees of The Science Museum for the layout, scale, appearance, landscaping, and access for the construction of Central Hall (F1 use class) including entrance hall, exhibition space and café with associated access, parking, landscaping, and external works following the demolition of the mess room and other structures pursuant to 18/01884/OUTM at the Railway Museum, Leeman Road, York.

The Head of Planning and Development Services gave an update noting that additional representations had been received, and the NRM response in respect of concerns raised regarding accessibility by Class 3 mobility wheelchairs/scooters. In their response the NRM confirmed that it would not differentiate between different classes of wheelchair and so Class 3 wheelchair users will be able to make use of the Walkway Route, including the passage through Central Hall. An advice note from Legal had been prepared for Members. The Head of Planning and Development Services also noted that written representations were attached to the speakers list for the meeting.

The Head of Planning and Development Services gave a presentation on the application noting that the principle of the closure of Leeman Road and alternative pedestrian and cycling routes was part of the outline planning permission and outline conditions. She added that the walkway agreement was a separate entity to the planning application. In response to a Member question regarding the applicant stating at the stopping up inquiry that the route would be determined as part of the reserved matters application, the Head of Planning and Development Services clarified that the walkway agreement is separate to planning. She clarified that there was the outline consent and condition 45 of the outline planning permission which requires detail of the walkway.

Public Speakers

Cllr K Taylor (Ward Cllr) spoke in objection to the application on behalf of residents. He referred to the number of objections to the scheme and asked the committee to defer the application.

David Finch (Chairperson of the Friends of Leeman Park) spoke in objection to the application on behalf of the Friends of Leeman Park. He detailed safety concerns regarding the riverside path and asked that a condition be included to state that construction at the NRM could not begin until the riverside route had finished. In response to a Member question, he noted that it was hoped that the riverside path would be widened.

Alice Williams spoke in objection to the application, explaining how the decision to approve would have detrimental effects on the residents of York. She expressed concern regarding a lack of engagement from the applicant and accessibility for all.

Christine Johnson spoke in objection to the application as a resident of St Peter's Quarter. She explained her concerns regarding safety, in particular to women due to the change in the route to St Peter's Quarter.

Ian Bissell, also a resident of St Peter's Quarter, spoke in objection to the application. He explained how those residents would be adversely affected by the plans. He noted that there was no equalities impact assessment (EIA).

Roger Pierce spoke in objection to the application on behalf of WalkYork. He suggested alternative walkways through the site and when asked, clarified what form these could take.

Jane Burton spoke in objection to the application on behalf of York Disability Rights Forum. She explained that the removal of the road route would significantly affect disabled people, particularly those reliant on taxis. She expressed concern regarding parking for blue badge holders and the lack of an EIA. In response to Member questions, she explained that:

- The lack of a designated pathway between the two doors was a problem for partially sighted people. They would not be able to use the route.
- Mobility cycles allowed freedom of movement and access and they would have to go around the NRM.

Clive Matthews spoke in objection to the application on behalf of York Cycle Campaign. He noted that there had been no EIA and that the plan curtailed pedestrian access and prohibit cyclists and would cause a serious loss of amenity with the greatest impact on vulnerable people. When asked about a workable solution, he suggested access around the Rijksmuseum in Amsterdam as a solution.

Cllr Melly (Ward Cllr) spoke in objection to the application on behalf of residents. She suggested that the conditions in the outline planning permission had not been met and that the benefits of the application did not outweigh the harm. She explained the harm caused by the application and that the benefits of the wider York Central development were not relevant to this application. In answer to Member questions, she explained:

- That there was no condition on the outline planning permission or stopping up order for improvements to the riverside route. She explained the two ways of accessing the route from St Peter's Square and the location of the island neighbourhood.
- The routes through the site including those that were and were not conditioned. She noted the route that traffic would take.
- That Ward Councillors had no input on the walkway agreement. She explained the number of times she had been in contact with the case officer.
- That the walkway agreement was shared for information at the stopping up order public inquiry. She listed the comments of the inspector at that inquiry.
- The outline planning permission included a freely available direct route, and the proposed route was not direct. The parameter plan for the outline planning permission showed a direct route.
- The Director of the NRM declined a meeting with Ward Councillors and MP.
- The route needed to be obvious and reasonably direct.

[The meeting adjourned from 17:55 to 18:03]

Sarah Loftus (Managing Director of Make it York (MiY)) spoke in support of the application on behalf of MiY. She explained that MiY welcomed the application. She added that the NRM is a world class attraction free to residents and visitors. On the visitor economy she noted that York attracted 8.4million tourists which brought income and created employment. She noted that the proposed development helped keep York relevant and the Central Hall would provide Learning and Development opportunities for people in the city.

Laurence Beardmore (President of the York and North Yorkshire Chamber of Commerce) spoke in support of the application on behalf of the Chamber. He noted that it was an aspiring plan. He noted that the plans would support railway heritage and signpost to the future. He added that the Central Hall plans had the support of businesses, and they would gain



as a result, and the plans were part of vision 2025 to become a world class visitor attraction and anchor for York Central. In response to questions from Member he explained that:

- The plans were a key part of York Central and would increase visitor numbers and jobs for the hospitality sector. It would also enhance the NRMs reputation by being a world class visitor attraction.
- The 1.2million extra visitors was based on NRM estimations.
- Regarding the York Central office space, not all businesses could hybrid work and there was a shortage of commercial space in York resulting in businesses moving away from or not coming at all because of the shortage. York Central would provide commercial space.

Natalie Webster (Homes England) spoke in support of the application on behalf of Homes England. She noted that the development would increase visitor numbers. She explained that Homes England had been working with York Central partners to ensure that there was comprehensive car parking across the site. Members asked several questions to which she responded that:

- There was multiple car parking provision as part of the York Central development.
- The commitment of Homes England was to deliver what had been included as part of the outline planning permission.
- Homes England were providing a highway route through the site.
- James Farrar (Local Enterprise Partnership (LEP) spoke in support of the application on behalf of the LEP. He explained that the proposals were economically important as they would improve tourism by attracting families. He noted that the cultural quarter would make York attractive to businesses. He added that the decision made by the Committee would send a clear message to developers regarding the vision for York Central. In answer to Member questions, he noted that:
  - The development would bring a new raft of people both domestic and international to York and would put York on the map again.
  - Regarding existing residents fitting in with placemaking for York Central it was his understanding that there would be widespread engagement regarding York Central.
  - How the development would bring in a developer interested in placemaking.

- He believed the plan put forward by the NRM was aspirational and would set the tone for that side of York.
- Driving innovation was about creating a place where people wanted to be.

Judith McNicol (Director of the National Railway Museum) spoke in support of the application on behalf of the applicant. She explained that the central hall would be a landmark entrance to York and York Central. She noted that it would safeguard the national collection, create jobs, and would act as a gateway. She explained that there had been consultation with neighbours and stakeholders and would create safer, greener, alternative routes for road users. She added that the central hall was being sensitively built, would help achieve net zero carbon and she noted the benefits of the railway futures gallery. She had a number of colleagues in attendance at the meeting to answer questions and in response to Member questions she and they explained that:

- Regarding the route through the site, there was a need to be cautious that the NRM was a national museum and the terrorism threat had to be thought about.
- At the outline planning application stage routes were considered and the decision was taken to close a section of Leeman Road. Condition 45 references the route through the site. It was noted that routes were available for car users, pedestrians, and cyclists and that the walkway route was an alternative route that was additionally available and was as direct as possible. There were two routes that more or less followed the route of Leeman Road and the location of these were explained.
- The original route for the walkway was longer and the proposal in the application before Members was to follow the route of the road. The deliberations of the public inquiry inspector were noted.
- From May 2023 the NRM would go to a seven day a week opening and there would be signage indicating routes through the site.
- Consultation was undertaken on different routes and going through the building was the best option.
- The walkway route would be available seven days a week when the museum was open.
- The NRM would not dictate how people behaved when moving through the site and it was hoped that residents would come through the central hall.

- There was not a direct route from St Peters Quarter.
- As part of the 2018 consultation the police were concerned that there wouldn't be a safe route to be open 24 hours a day. The 2018 consultation looked at seven options with residents and it was explained why other options were not feasible.
- The problem with using the existing underpass was explained. The priority for the NRM was the safe movement of residents and visitors. They had looked at a design that worked for all people, with flat access.
- There was an access consultant for the 2025 vision and a lot of work had been undertaken on the open for all strategy. A living person user group had been recruited and they had looked at the open for all strategy.
- There would be a way finding team for the central hall and the importance of staff availability to support blind and partially sighted people through the great hall was noted.
- The Equalities Impact Assessment was the responsibility of the council.
- The route from Leeman Road to Marble Arch and the location of the tactile crossings on the route was explained. The entry and exit doors were also explained.
- Electric vehicle (EV) charging was provided at two of the 14 disabled parking spaces and there would be points across the site. It was not known how many EV charging points there would be until other applications came forward.
- The two EV charging points would be completed by Autumn 2025. The infrastructure for EV would not be put into the 14 disabled parking spaces but there would be cabling along those spaces.
- The work being undertaken on historic buildings was noted and the history of the mess room was explained, noting that it was not listed and that retaining it would sever the routes across the site.
- The concerns of the police designing out crime officer regarding bollard lighting had been superseded.
- Cyclists, including mobility cycles could not pass through the site.
- How the projected visitor numbers had been estimated.

- The travel plan was a supporting document in the application, and it covered staff and visitors. Free staff parking was being addressed by the museum.
- A number of planning applications for the site had already been delegated.

[The meeting adjourned from 19:43 to 19:55]

The Chair reminded Members of the officer recommendation. In response to questions to officers it was clarified that:

- It was not known how overlooked the new route on Leeman Road would be and there would be street lighting along that route.
- That the Committee were considering the walkway route and access arrangements, not the walkway agreement. The Senior Solicitor was asked and clarified that a walkway agreement dealt with the footway over or through a building, and that there was no legal requirement to consult on a walkway agreement.
- Comments from highways officers that referred to internal operations were not in the scope of the reserved matters application.
- The application, including the area to be considered by the Committee was clarified.
- The new route through the site was a longer and safer route through the infrastructure. The committee had to consider the application before it and whether the arrangements in the application were acceptable.
- The pathways would be part of future reserved matters applications. Museum Square would be an open space and the detail of the layout of the square would be part of a different application.
- The routes agreed as part of the outline planning application were clarified.
- Officers did not have the detail of the surfacing as at that stage, the space needed to remain open for access.
- There was no Equalities Impact Assessment (EIA). The public sector equalities duty applied to all council business and planning officers had to consider it. An EIA could be used to evidence it, but it could be evidenced in different ways. As part of their assessment, officers had looked at the impact on disabled people including gradients, widths, and disabled parking.

- The detail on car parking was included in the conditions. The multistorey car park was earmarked for the NRM.
- In the reserved matters application plans showed guard rails along the south side of the route to the crossing.
- The Senior Solicitor confirmed that the walkways agreement could be amended and that this had been delegated to officers by the Executive. It was further clarified that, officers in consultation with the Executive Member could amend the walkways agreement but that it was not a matter for consideration by this committee.
- It was confirmed that the applicant could apply for non-determination the following Monday.

Following debate Cllr Ayre moved the officer recommendation to approve the application. This was seconded by Cllr Fisher. On being put to the vote with five Members voting in favour and six against, the motion fell.

Further debate followed, and in relation to the EIA Members were advised that equalities had been taken into account through the design and access statement. Cllr Pavlovic moved deferral of the application for an EIA to be undertaken. This was seconded by Cllr Barker. On being put to the vote with seven Members voting in favour and four voting against, deferral of the application was carried, and it was decided;

Resolved: That the application be deferred for an Equalities Impact Assessment to be carried out by officers in respect of the walking route shown edged yellow on Drawing Number 201564\_NRM\_OP\_SW\_0001 Rev 05.

Reason: In order to take into account the needs of people with protected characteristics.

Cllr Cullwick, Chair

[The meeting started at 16:30 and finished at 21:29]

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Meeting	Planning Committee A
Date	11 July 2022
Present	Councillors Cullwick (Chair), Pavlovic (Vice-Chair), Ayre, Barker, D'Agorne [until 17:09], Kilbane, Fisher, Looker and Daubeney (Substitute for Cllr Melly)
In Attendance	Gareth Arnold (Development Manager) Sandra Branigan (Senior Solicitor) Alison Stockdale (Development Management Officer)
Apologies	Councillors Waudby, Doughty and Melly

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**10. Declarations of Interest****17:08**

The Chair opened the meeting by explaining that it was being held at the Grand hotel as the council offices (West Offices) were temporarily closed. He expressed gratitude to the Grand for the use of its board room. A Member sought assurances over the lawfulness of the meeting and the Chair stated that he had received assurances that the meeting was legal.

Members were asked to declare, at this point in the meeting, any personal interests, not included on the Register of Interests, or any prejudicial or disclosable pecuniary interests they may have in respect of business on the agenda. Cllr D'Agorne noted that Lars Kramm, a registered speaker for the planning application was on the Green Group. He also noted that as Executive Member for Transport he had made a decision on the replacement bridge in Copmanthorpe and as such there may be a preconception that he was predetermined on the application and he would therefore leave the meeting for that item. No further interests were declared.

[Cllr D'Agorne left the meeting at 17:09]

**11. Public Participation****17:09**

It was reported that there had been no registrations to speak at the meeting under the Council's Public Participation Scheme on general matters within the remit of the Planning Committee.

**12. Plans List**  
**17:10**

Members considered a schedule of reports of the Head of Planning and Development Services, relating to the following planning applications, outlining the proposals and relevant policy considerations and setting out the views of consultees and officers.

**2a) OS Field Lying to the South of and Adjacent to no 1 Tadcaster Road, Copmanthorpe, York [18/00680/OUTM] 17:10**

Members considered a major Outline planning application from Mr Gladman with all matters reserved except for means of access for the erection of 158no. dwellings with public open space, landscaping and drainage at OS Field Lying to the South of and adjacent to no 1 Tadcaster Road, Copmanthorpe, York.

The Development Manager gave a presentation on the application detailing the site allocation in the draft Local Plan and the very special circumstances that would clearly outweigh any harm to the Green Belt. The presentation demonstrated the site location, aerial photographs, the development framework plan, housing types and photos across the site.

A committee update was given and Members were informed of a change to paragraph 5.12 in the report, and two further letters of objection from local residents, additional consultation responses from ecology following pipistrel bat day roosts being found in tree T22, amended recommendation to refer to the Secretary of State, additional conditions relating to the recreational impacts on Askham Bog and amended conditions 2, 7, 15, 23 and 29.

Public Speakers

Lars Kramm spoke on behalf of Copmanthorpe Parish Council. He noted that the Parish Council did not object to the application and the site was one of two sites identified by residents for development and it was included in their draft Neighbourhood Plan. He asked the committee to approve 75 dwellings. He requested that the application be deferred citing concerns and suggested changes regarding access proposals. He also requested passivhaus standards for the dwellings.

Chris Lee spoke in support on behalf of the applicant. He thanked the case officer for her work on the application. He addressed the very special circumstances outweighing any harm to the Green Belt. He noted that the applications would provide 111 market dwellings, and if approved the site would be sold quickly to a developer. He advised that the development



would deliver 47 affordable homes and would enable the delivery of a key strategic site. He noted that 44% of the site would be open space, the economic benefits of the development, opportunity to delivery of biodiversity and contributions in excess of £2million. In response to Member questions he confirmed that:

- The play area would have play equipment and it was preferable for the maintenance of it to be taken on by a management company paid for by the residents.
- Regarding the play area being near the railway, it had been moved further north so that it was further away from the proposed railway crossing.
- The plans for the play area, sustainability and heating system would be submitted as part of the reserved matters application.
- Regarding a contribution to the bridge over the crossing, the level crossing had not been fixed and this was a pre-existing issue not directly caused by the development.
- The request for the mix of affordable housing came from the council.
- The market housing mix had not been made at this stage and representations could be made to the developer when the reserved matters application came forward.

Members then asked questions to officers, to which officers clarified that:

- The local plan suggested 35 dwellings per hectare in rural settings and the application was five dwellings over that figure.
- The density did not need to include open space.
- ST31 in the draft Local Plan included 138 houses.
- The density in the neighbouring development was 35 to 40 dwellings per hectare
- Calculations for contributions were made according to S106 calculations.
- The Senior Solicitor clarified that there needed to be a project for the S106 healthcare contribution. The money from the S106 agreement would usually have to be spent within a set period or else it would need to be paid back.
- The request for the S106 contribution for healthcare was made by the CCG and the need for an additional dental surgery was not identified during consultation.
- 20% affordable housing was required on this greenfield site of which 80% should be social rented

Following debate Cllr Pavlovic proposed the updated officer recommendation to delegated authority to the Head of Planning and Development Services to APPROVE the application subject to referral to the Secretary of State under The Town and Country Planning

(Consultation) (England) Direction 2021 and the completion of a Section 106 Planning Obligation. This was seconded by Cllr Fisher. On being put the vote with Members unanimously voting in support

Resolved: That delegated authority be given to the Head of Planning and Development Services to APPROVE the application subject to:

**1. Referral to the Secretary of State under The Town and Country Planning (Consultation) (England) Direction 2021**

2. The completion of a Section 106 Planning Obligation to secure:

i The provision of 48 affordable housing units

ii **Open space and play areas** management plan

iii £1,347,568 toward the provision of Early Years, Primary and Secondary school places

iv £102,240 towards off site sports provision

v Between approximately £199,698 and £291,264 towards primary healthcare provision (doctors' surgery)

vi Provision of dropped crossings on Millers Croft, Flaxman Croft, Saddlers Close and Sutor Close (£8,800); and changes to the speed limit location on Tadcaster Road (£5,000) through a Traffic Regulation Order

vii Provision of a £200 per dwelling contribution towards a public transport pass or cycling equipment to be awarded to the first occupier, and £100 per dwelling to be awarded to the first occupier towards car club incentives, up to a total of £40,000

viii Travel plan co-ordinator £32,500 for 5 years and another £32,500 if target is not met

ix £66,000 to upgrade Yorkfield Lane from site to Ploughman's Lane

x £66,000 to upgrade track from Farmer's Way to school

xi £40,000 towards A1036/ Sim Balk Lane signal controller update

**xii Mitigation measures for recreational impacts on Askham Bog SSSI – Replacement of entire wooden boardwalk at Askham Bog with longer life recycled plastic option £300,000, and £20,400 towards engagement work with the residents on the development, starting from first occupation and continuing for 3 years in total.**

All contributions to be index linked.

3. The conditions set out below, and

The Head of Planning and Development Services be given delegated authority to finalise the terms and details of the Section 106 obligations and conditions.

Additional conditions

1. Details of play space and play areas to be submitted and approved in writing prior to commencement. Details to include means of enclosure, equipment and area of play space to be delivered in accordance with policy GI6. Approved play areas to be delivered in accordance with a time scale to be approved in writing and retained for the lifetime of the development unless otherwise approved in writing by the LPA through the submission of a planning application.

2. The development shall not be begun until full details of a connection between the internal cycle / pedestrian paths and Yorkfield Lane and the internal cycle / pedestrian paths and the existing cycle/pedestrian path on Tadcaster Road have been submitted and approved in writing by the Local Planning Authority. The adjacent phase of the development shall not be first occupied until the relevant connection has been constructed in accordance with the approved plans unless an alternative trigger point has been approved in writing by the LPA.

3. The felling of the Ash tree (T22) shall not commence unless the local planning authority has been provided with either:

- a. a licence issued by Natural England pursuant to Regulation 53 of The Conservation of Habitats and Species Regulations 2017 (as amended) authorising the specified activity/development to go ahead; or
- b. Confirmation that the site is registered on a Bat Mitigation Class licence (formally Low Impact Class Licence) issued by Natural England; or
- c. a statement in writing from the relevant licensing body to the effect that it does not consider that the specified activity/development will require a licence.

Reason: To ensure bats are protected from harm during demolition works. All British bat species and their roosts are protected by the Wildlife and Countryside Act 1981 (as amended) and the Conservation of Habitats and Species Regulations 2017 (as amended).

4. Prior to first occupation, details of measures to mitigate for the loss of the ash tree (T22), and to provide enhancements, in relation to bat habitat to be submitted and approved in writing by the Local Planning Authority. Approved details to be implemented in accordance with an approved timescale.

#### Amended conditions

2. 'Building heights plan Fig 5.8' replaced by '**Building heights plan July 2022**'.

7. Prior to commencement of construction of the development, or phase of development, a detailed landscaping scheme shall be submitted to and approved in writing by the Local Planning Authority and the development shall be carried out as approved. The scheme shall adhere to the principles of the approved parameter plans and the open space provision and shall detail-

- a) A planting plan showing a minimum of 10 replacement trees, **of a minimum size of 10-12cm girth (measured at 1m) and 3 metres in height**, as mitigation for the loss of ash tree T22 as indicated on the Tree Retention & Removal Plan [BHA\_316\_02, CSA, March 2018], **and in addition to the trees required under (b) and (c)**.
- b) The number, species, stock size / height and position of trees and shrubs.
- c) The provision of street trees throughout the residential development area.
- d) Location and specification of street furniture / seating within public open spaces.
- e) Management and maintenance.

Any trees or plants which die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of a similar size and species, unless alternatives are agreed in writing by the Local Planning Authority.

Reason: In accordance with NPPF sections 8 and 12. So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of species within the site in the interests of the character and appearance of the area and to ensure adequate play facilities for future residents and in the interests of railway safety.

15. To be deleted as covered under condition 17.

23. To be deleted as now covered under Building Regulations.

29. Before or concurrently with the first application for the approval of reserved matters, a strategy for the development of at least 5% self or custom build plots across the whole site shall be submitted to and approved in writing by the Local Planning Authority. This strategy shall include a design code setting out the following details:

- Appearance
- Landscaping
- Layout
- Scale

**It shall also align with the general phasing of the site and include the means of marketing and time frames for delivery of the plots.**

The self and custom build housing shall be provided with services (access to a public highway and connections for electricity, water and waste water) to the extent that it can be defined as a serviced plot of land, as defined in The Self-build and Custom Housebuilding Regulations 2016.

The development of the self and custom build dwellings hereby approved shall not be carried out unless as 'self-build or custom-build' development as defined in the Glossary in Annex 2 of the National Planning Policy Framework (February 2019) or any subsequent replacement document.

**Where dwellings/plots have been appropriately marketed for at least 12 months, in accordance with the approved details, and have not sold, the relevant plots may be built out as conventional plots for market housing by the developer.**

All applications for approval of reserved matters for the self-build dwellings shall be in accordance with this strategy and other approved details.

Reason: In order that the Local Planning Authority may be satisfied as to the details of the development and to comply with the Town and Country Planning (General Development Procedure) (Amendment) (England) Order 2006 and in the interests of local housing need.

Additional informatives

- Enhanced energy efficiency
- Segregated cycle link through site
- Location of play area

Reasons:

- i. The report outlined how the proposed development, subject to conditions, can be compliant with the NPPF with regards to impacts upon the highway network, sustainable travel, residential amenity, archaeology, biodiversity, landscape, flood risk and drainage. In addition to this there were considered to be suitable mechanisms to ensure that the infrastructure required to support the development could be secured.
- ii. At present the site was considered to remain within the general extent of the Green Belt. However as is set out above, it was identified as part of the portfolio of sites to meet identified needs in the city and is therefore excluded from the green belt in the defined green belt

boundaries. It was considered that there are very special circumstances that would clearly outweigh any harm to the Green Belt. Further, there is no case for refusing the scheme on prematurity grounds.

- iii. Based on the merits of the case it was recommended that planning permission be granted subject to conditions and completion of a Section 106 Agreement.

Cllr Cullwick, Chair

[The meeting started at 17:00 and finished at 18:05].



- c) encourage persons who share a relevant protected characteristic to participate in public life or in any other activity in which participation by such persons is disproportionately low.

- 0.4 The PSED does not specify a particular substantive outcome, but ensures that the decision made has been taken with “due regard” to its equality implications.
- 0.5 Officers have given due regard to the equality implications of the proposals in making its recommendation, however this has now been documented in an Equalities Impact Assessment. This assessment, which pulls together information contained within the submission documents of the application, is attached at Appendix 1 for Members’ information.
- 0.6 Members are also updated with respect to the latest comments which were received from North Yorkshire Police:

*It is pleasing to note that the northern pedestrian route is now part of the access road and that it will be illuminated for the majority of its length by column lights. It is considered that this arrangement provides fewer opportunities for crime and generally increases safety.*

- 0.7 Members are also updated with respect to comments received from York Civic Trust which were included in the Committee Update of 7 July 2022 and are summarised as follows:

York Civic Trust is supportive of the principle of Central Hall as a connection between the two exhibition halls and a solution to the identified issues of access and connectivity across the museum.

There is opportunity to revise the scheme in both internal layout and size and scale of the central drum which otherwise limit the ambition of the building and its potential to become a large statement building.

Current design offers unacceptably poor through access from Leeman Road/Salisbury Road and fails neighbouring communities.

Impact of the central drum will be a defining feature and landmark for both the NRM and wider York Central. As the eventual gateway to York Central from the railway station it will be in a highly prominent position and needs to aspire to become a York icon with a lasting contribution to the skyline. Enlargement of size and scale of the drum would help realise this.

- 0.8 These comments are noted by Officers, matters relating to access, layout and design are covered within the Committee Report at paragraphs 5.8 and 5.9 and 5.72, 5.74 and 5.75.



0.09 Members are also reminded of the comments by the Applicant included in the Committee update of 7 July 2022 with respect to the route through the Museum site as follows:

With respect to concerns raised regarding accessibility by Class 3 mobility wheelchairs the following comments have been received from the NRM:

The National Railway Museum have confirmed that it will not differentiate between different classes of wheelchair and so Class 3 wheelchair users will be able to make use of the Walkway Route, including the passage through Central Hall.

When using the Walkway Route, or travelling inside the museum, Class 3 wheelchair users will need to moderate the speed at which they operate the devices to 4 mph or less. This is for the safety of all users of the route and is understood to be in line with DfT guidance for the operation of Class 3 wheelchairs in pedestrian areas.

As it stands, the Walkway Agreement does not prohibit the use of route by Class 3 wheelchair users. If however, the Council thought it necessary to include some additional wording to clarify the above position then NRM would be happy to incorporate this.

These comments have been made to provide clarity, however relate to the Walkway Agreement rather than the reserved matters for consideration.

0.10 The original Committee Report is presented below:

## **1.0 PROPOSAL**

1.1 The proposal relates to a reserved matters application for the construction of a Central Hall at the National Railway Museum (NRM), including entrance hall, exhibition space and café with associated access, parking, landscaping and external works following the demolition of the former mess room and other structures. The application seeks consent for layout, scale, appearance, landscaping and access.

1.2 This application follows outline approval 18/01884/OUTM for the redevelopment of York Central to provide a mixed-use development of up to 379,729 m<sup>2</sup> of floorspace Gross External Area (GEA) primarily comprising up to 2,500 homes (Class C3), between 70,000 m<sup>2</sup> and 87,693 m<sup>2</sup> of office use (Class B1a), up to 11,991 m<sup>2</sup> GEA of retail and leisure uses (Classes A1-A5 or D2), hotel with up to 400 bedrooms (Class C1), up to 12,120 m<sup>2</sup> GEA of non-residential institutions (Class D1) for expansion of the National Railway Museum, multi-storey car parks and provision of community uses all with associated works including new

open space, ancillary car parking, demolition of and alterations to existing buildings and associated vehicular, rail, cycle and pedestrian access improvements.

- 1.3 The reserved matters application site relates to Development Zone G and more specifically Character Zone 13 (Museum) as set out in the York Central Approved Parameter Plans and Design Guide.
- 1.4 The application proposes demolition of the existing entrance and lean to building, the former mess room which is a later extension to the Bullnose building, the underpass between Station Hall and Great Hall and associated plant, service and temporary buildings adjacent to Great Hall and the removal of existing portacabins adjacent to the Learning Platform building.
- 1.5 Following demolition, the construction of Central Hall will provide a new 3542sqm entrance building with associated visitor facilities to include exhibition gallery space, a new shop and café. The new building will comprise a central drum set over two stories which will sit higher than the existing buildings to be demolished and will comprise an internal viewing balcony at first floor level. The central drum would step down to a new 'futures gallery' to the west connecting it to Station Hall and a new café and shop to the north east which would connect through to the Great Hall. The new buildings will provide level access throughout and will unify the buildings on the existing NRM complex.
- 1.6 It is proposed that the main entrance will be located on the southern side of the building from Museum Square which is being developed as a later phase of York Central to be delivered by Homes England/Network Rail. The north eastern side of the building will provide a secondary entrance leading from Leeman Road.
- 1.7 Associated landscape improvements will be provided to the north eastern part of the site and will accommodate a service road, 14 accessible disabled car parking spaces and a pedestrian route including both stepped and ramped accesses set within a new soft landscaped space.
- 1.8 The proposed Central Hall is proposed to unite the National Railway Museum estate, becoming the main cultural focus for the wider York Central development and is described as the cornerstone of the Museum's 'Vision 2025'. 'Vision 2025' is a wider masterplan for the Museum involving a number of elements aside from Central Hall, the key components being South Yard, intended as an active public realm including events and play spaces and Wonderlab a new interactive gallery to be located in North Shed. These elements will be brought forward under future applications.

- 1.9 It is intended that the application proposals would increase visitor numbers to the museum from 750,000 to 1 million visitors per annum. The application suggests that the proposals could generate an additional 35 staff and opportunities for up to 150 additional volunteers/casual staff.
- 1.10 A listed building consent application for demolition of those buildings attached to existing listed buildings accompanies this application and is referenced 22/00156/LBC. This application is pending determination.
- 1.11 The outline planning application for York Central required an Environmental Statement as well as a number of detailed technical assessments which set out the anticipated environmental impacts arising from the development of York Central including the Museum site. An Environmental Compliance Statement is submitted as part of the Planning Statement submitted for this reserved matters application to demonstrate that the proposals would not result in any new or material environmental impacts from those identified and approved at outline stage and as such any mitigation measures outlined still remain relevant. In addition, it is confirmed that the proposals fall within the design parameters set out within the Design Guide and Parameter Plans approved at outline stage. Of particular note is the confirmation that the floor area of the proposed building has reduced significantly since that envisaged at outline stage. In addition the proposals sit within the limits of deviation in respect of access and circulation routes and with respect to proposals being within the maximum building heights.

### **APPLICATION SITE**

- 1.12 The application site forms part of the National Railway Museum complex which sits astride Leeman Road with the main museum buildings (the Great Hall and Station Hall) linked by private underpass beneath Leeman Road. The site lies to the west of the City centre and York Railway Station. The site currently comprises a number of existing museum buildings including a number of Grade II listed buildings comprising Station Hall, Peter Allan Building, Bullnose Building, Weighbridge, Gate Piers and Gates to the former York Goods Station which will remain in situ. The site falls outside of the Central Historic Core Conservation Area which is located to the east and includes the city walls - a scheduled monument.
- 1.13 The modern housing development of St Peter's Quarter, off Leeman Road sits beyond the west of the site and comprises three and four-storey town houses and apartment blocks.
- 1.14 Leeman Road runs through the Museum site which currently provides vehicular, pedestrian and cycle access for existing residents into York City Centre.

## BACKGROUND AND RELEVANT PLANNING HISTORY

- 1.15 The redevelopment of York Central within which the NRM application sits has been in discussion for decades following the decline of the rail industry. Complexities of land ownership had prevented a comprehensive redevelopment scheme coming forward at an earlier stage.
- 1.16 In 2019 outline planning approval (18/01884/OUTM) was granted for redevelopment of the site to provide a mixed-use development of up to 379,729 m<sup>2</sup> of floorspace Gross External Area (GEA) primarily comprising up to 2,500 homes (Class C3), between 70,000 m<sup>2</sup> and 87,693 m<sup>2</sup> of office use (Class B1a), up to 11,991 m<sup>2</sup> GEA of retail and leisure uses (Classes A1-A5 or D2), hotel with up to 400 bedrooms (Class C1), up to 12,120 m<sup>2</sup> GEA of non-residential institutions (Class D1) for expansion of the National Railway Museum, multi-storey car parks and provision of community uses all with associated works including new open space, ancillary car parking, demolition of and alterations to existing buildings and associated vehicular, rail, cycle and pedestrian access improvements.
- 1.17 The outline approval was submitted with an Environmental Statement and was subject to 83 conditions together with a Section 106 agreement. The highway and transport impacts arising from the development were assessed in a series of detailed Transport Assessments including traffic modelling. These were undertaken on the basis that a section of Leeman Road would be stopped up for traffic with a potential pedestrian connection which 'could be through a building' along the former Leeman Road (Outline Parameter Plan YC-PP-006).
- 1.18 The first of the reserved matters applications under reference 20/00710/REMM was granted in November 2020. That application sought consent for layout, scale, appearance, landscaping and access for the construction of the primary vehicle, pedestrian and cycle routes and included associated landscaping and alterations to the existing road network pursuant to outline planning permission 18/01884/OUTM. This consent secured the new vehicular, pedestrian and cycle routes through York Central which include alternative routes to Leeman Road as referred to in Condition 45 of the outline consent.
- 1.19 As a separate process, under the Highways Act 1980, the Applicants sought a Stopping Up Order (SUO) in order to permanently remove highway rights from a 220m section of Leeman Road. Homes England and Network Rail were granted a Stopping Up Order by the Department for Transport which came into effect on 6 October 2021 and is subject to a series of stipulations which must be complied with. The SUO enables a section of Leeman Road to be removed as public highway and will come into operation once the Local Highway Authority is satisfied that the conditions of the Order have been met. Prior to the stopping up coming into force, alternative vehicular, pedestrian

and cycle routes must be available around the stopped up length of Leeman Road.

- 1.20 In line with the outline consent, in addition to the new and improved public highway routes consented under 20/00710/REMM, pedestrians will also be able to pass through the Railway Museum during museum opening hours. The detail of the layout of the site and access is subject to this reserved matters application, however the detailed arrangements for the operation of the route through the museum are subject to a Walkway Agreement under Section 35 of the Highways Act 1980. The purpose of a Walkway Agreement being to dedicate footways in, through or under parts of a building for use as a footpath. The Walkway Agreement was approved on 27 April 2021 during the Stopping Up Public Inquiry.
- 1.21 The existing Walkway Plan accompanying the Walkway Agreement shows a different alignment to that shown on this reserved matters application and as such the Walkway Plan will need amending so that it aligns with the reserved matters plans. In addition Condition 45 will need to be subsequently discharged, this being the mechanism by which the Council are able to control details (for example alignment, width, gradient and surfacing) of the pedestrian access through Central Hall.
- 1.22 A number of pre-commencement conditions pertaining to the outline consent and reserved matters application for the infrastructure works have already been discharged and a series of additional applications are anticipated over the coming months. The discharge of these conditions will enable lawful commencement of the infrastructure works and alternative pedestrian and cycle routes through York Central to commence later this summer.
- 1.23 A number of other planning consents for improvements to the museum, which tie into the wider regeneration of the site, have recently been consented, the most relevant are as follows:
- 21/02544/FUL Erection of single storey workshop with associated external works Granted 13.01.2022
  - 21/02484/FUL Works to southern elevation of North Shed to include removal of loading crane from service yard, removal of folding doors and infilling with metal cladding and glazing, installation of 2no. mechanical grilles, replacement of roller shutter door with solid double doors and formation of new doorway Granted 05.01.2022.
  - 21/02379/FUL Replacement of roofing at Station Hall including rooflights, re-opening four western rail access arches and two southern arches with

installation of glazed screens and removing modern baffle walls Granted 21.12.2021

- 21/01882/FUL Replacement of existing roof covering at Learning Platform building Granted 04.10.2021

## **PRE-APPLICATION ENGAGEMENT BY APPLICANT**

- 1.24 The Applicants have submitted a Statement of Community Involvement (SCI) which sets out in detail the community engagement that has been undertaken prior to the reserved matters submission.
- 1.25 In summary, the engagement involved contacting key stakeholders and politicians, making local community groups aware of the consultation, a leaflet drop to 7,000 local residents and media publicity providing details of a public exhibition and consultation website together with contact details for queries. An exhibition was held at the National Railway Museum from Monday 25 October 2021 to 3 November 2021 with feedback being obtained online through a dedicated consultation webpage. There were 93 responses and these are summarised in the SCI.
- 1.26 As well as the engagement with the local community and key stakeholders the Applicants had various pre-application meetings including discussions with Planning, Conservation and Highways on the lead up to the submission of this application.

## **2.0 POLICY CONTEXT**

- 2.1 National Planning Policy  
The revised National Planning Policy Framework (NPPF) was published in 2021 and its planning policies are material to the determination of planning applications.
- 2.2 Development Plan  
Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that determinations be made in accordance with the development plan unless material considerations indicate otherwise.
- 2.3 The Publication Draft City of York Local Plan 2018 (the emerging plan) was submitted for examination on 25 May 2018. Phase 1 of the hearings into the examination of the Local Plan took place in December 2019 with further hearings held in May 2022.
- 2.4 The key policies relevant to the proposals are:

DP1 – York Sub Area

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DP2 – Sustainable Development  
DP3 – Sustainable Communities  
DP4 – Approach to Development Management  
D1 – Placemaking  
D2 – Landscape and Setting  
D5 – Listed Buildings  
D6 – Archaeology  
D7 – The Significance of Non-designated Heritage Assets  
D10 – York City Walls  
GI2 - Biodiversity  
GI4 – Trees and Hedgerows  
CC1 – Renewable and Low Carbon Energy  
CC2 – Sustainable Design and Construction of New Development  
ENV1 – Air Quality  
ENV2 – Managing Environmental Quality  
ENV3 – Land Contamination  
ENV4 – Flood Risk  
ENV5 – Sustainable Drainage  
WM1 – Sustainable Waste Management  
T1 – Sustainable Access  
T7 – Minimising and Accommodating Generated Trips  
T8 – Demand Management

2.5 The following policies are also relevant to the planning application. They have outstanding objections but are consistent with national policy and can therefore be given limited weight (the objections will be considered through the Local Plan Examination process).

SS4 - York Central

2.6 Evidence Base

The evidence base underpinning the emerging plan is also capable of being a material consideration in the determination of planning applications.

2.7 The City of York Draft Local Plan Incorporating the Fourth Set of Changes was approved for Development Management purposes in April 2005 (DCLP). Whilst the DCLP does not form part of the statutory development plan, its policies are considered capable of being material considerations in the determination of planning applications where policies relevant to the application are consistent with those in the NPPF as revised in 2021, although the weight that can be afforded to them is very limited.

2.8 In terms of site constraints then the following apply:

- The wider site has been designated as a Housing Zone and has also been awarded Enterprise Zone status.
- The site is located in an Area of Archaeological Interest.
- The site contains a number of Grade II Listed buildings including Station Hall, Peter Allan Building, Learning Platform Building (curtilage listed), Bullnose Building, weighbridge, Gatepiers and Gates to former York Goods Station.

### 3.0 CONSULTATIONS

#### Internal

#### 3.1 Policy

The policy position has been approved and this application does not seek to challenge the principles or position therefore there is no objection.

#### 3.2 Highways Development Control (HDC)

##### Walkway route

The application proposes a new alignment for the walkway agreement route through the Museum. This alignment is more direct than the previous proposal and generally considered suitable for users in terms of width and gradient (including the ramp option) and is considered to be compliant with the Walkways Agreement design requirements but the plan attached to the agreement will need to be changed to match the RMA layout.

HDC previously raised the issue of people wanting to walk through the museum having to join queues of visitors. The number of people walking through the museum at peak visiting times (10am to 11am) are estimated at between 20 to 30. HDC remain opposed to this approach as this is against the spirit of the outline application and Walkway Agreement which should provide free passage through the Museum during opening hours for those who are simply travelling through the area and not visiting the museum. The walkway route is required to provide a direct route through the site and suggesting people travel around the site if there are queues goes against the objectives of the Walkway Agreement and the outline consent. As previously requested, a separate entrance/check point should be provided to ensure they do not have to queue.

There is also need for closure notification signs at the east approach to minimise abortive walking distance for non-visitors. The location for signs at the west gates have been included but not at the east approach.

##### Cycle Parking

Staff cycle parking and visitor parking provision is acceptable in principle but a summary showing total numbers of spaces and how many stands/spaces are provided for staff adjacent to the security entrance should be conditioned.



### Car Parking

General car parking will rely on car parking provision on the wider site (multi storey and temporary car park). According to the Transport Assessment 184 visitor parking spaces and approximately 70 staff car parking spaces will be removed with car users expected to switch mode or use multi storey car parks on site. Condition 12 of the OPA requires a detailed phasing strategy for car parking, before the commencement of any development. We will therefore rely on this condition to ensure that adequate parking is available for the Museum at all times. Through this condition, we will need to continue to consider the risk of increase in car parking from NRM visitors and staff in adjacent streets which are not currently covered by ResPark.

### Train drop off and bus layby

The proposed development relies on the provision of the train drop off layby and bus layby as well as access for deliveries which are not within the red line and depend on the implementation of 20/00710/REMM. The provision of these facilities need to be completed before the proposed development is occupied. The applicant notes that “these works fall under the RMA for the proposed infrastructure, which needs to have been completed prior to the closure of Leeman Road. As such there is already an appropriate mechanism in place and no further conditions are required.”

The Leeman Road closure does not require all the works in the York Central Highway Infrastructure RMA to be completed for Leeman Rd to be stopped up. HDC consider that the train drop off layby must be in place prior to the NRM opening so would require this to be conditioned.

### Travel Plan and Transport Assessment

HDC note the updated Travel Plan and Transport Assessment and have no further comments.

### Construction Management

Note that a Construction Management Plan is part of Condition 15 of the outline consent and would need discharging prior to commencement.

## 3.3 Economic Development Unit

The NRM has been an integral part of York’s heritage and a major asset to the City’s cultural offer since 1975. Not only is the site a museum and major tourist attraction, it also houses rare collections of rolling stock, artefacts and ephemera spanning 200 years of railway history, cementing York’s status as an important ‘Railway City’.

Over the last 47 years the venue has attracted 33 million visitors from across the world. This is 750,000 visitors per annum, 17% of whom live locally. The application proposes a very large extension to the existing museum

infrastructure, offering a new visitor 'welcome' space, an exhibition space – the Futures Gallery, café and the main Central Hall. Offering state of the art facilities will inevitably increase footfall, projected to be 1 million visitors per year, all of whom will be visiting the city for one day, many for longer, boosting the local economy particularly the hospitality and cultural sectors.

Inevitably the increase in building size and variety of specialist areas will create new jobs as well as securing the long-term future for people already employed by the Museum.

The educational benefits of the NRM are noteworthy, actively encouraging interest in STEM subjects and the proposed Wonderlab will allow children aged between 7 and 14 to participate in engineering workshops, helping nurture future generations of talent some of whom will be of key benefit to burgeoning sectors already established in York.

The NRM is also integral to the development of York Central, complementing and enhancing the unique qualities of the project.

Given the economic significance provided by this proposed expansion we support this application.

#### 3.4 Lead Local Flood Authority

No objection in principle on the basis that our interests are covered by conditions imposed on the outline planning permission (18/01884/OUTM) Advise that it is ensured that details submitted at reserved matters stage would not prejudice the developer's ability to meet the requirements of those conditions.

#### 3.5 Urban Design and Conservation

##### Original Comments

Are generally supportive of the proposals and consider the design of the new buildings to be of the highest quality. The scale, massing and design all make a positive contribution to most of the site's heritage assets. There are concerns about treatment of the bullnose building. The application needs to provide justification for the demolition of the mess building which is an acknowledged non-designated heritage asset of some architectural interest, historic interest and contributes to group value of the railway complex.

##### Latest Comments

The proposal scheme results in the total loss of significance of the mess room, a non-designated heritage asset. The Applicants acknowledge this would result in harm to the setting and significance of the grade II listed Bullnose Building. Whilst of a later date, the mess room visually and as a component of the inter-related functions of the buildings, contributes to the setting of the

group of the designated heritage assets.

The design document suggests that the removal of the building is required to better reveal the new entrance frontage and link two public spaces which will be created as part of the scheme. However the impact on the frontage is limited in wider views of the site, and visitors would in any case be drawn by the prominent new central drum. Whilst demolition would allow the creation of a single larger public space, it would result in the bullnose building appearing isolated, diminishing its group value and that of the bullnose building and the buildings within which it forms a group, and the total loss of significance of the non-designated heritage asset. Conservation Officer's opinion is that this is insufficient justification for the loss of significance.

### 3.6 Public Health

No response received.

### 3.7 Trees and Landscape

#### Original Comments

There needs to be a review of the hard surfacing along the length of the old Leeman Road, in order to play down the black asphalt and its old form and function and to integrate the street better with the pedestrian use and give a greater sense of celebratory arrival. The soft landscaping principles are fine but there needs to be a greater number and variety of tree species, including some large species, to reflect the scale of the overall development.

#### Latest Comments

The amended landscaping plan looks fine and seems to be as discussed at the meeting with the Applicants. If all the disabled parking bays are required then what is now proposed is satisfactory and the planting detail is accepted.

### 3.8 Ecology

Low level lighting has been designed alongside soft landscaping which should limit impacts on light sensitive species, by ensuring foraging and commuting habitat is available for such species. It would be useful if a contour plan could be provided to show light levels and light spill which can then be reviewed to ensure existing and proposed green areas and corridor will provide suitable habitat for nocturnal species.

With respect to removal of hedgerows, trees, shrubs and demolition of buildings or structures, where habitat is to be lost during the nesting bird period, checking surveys are required by an ecologist.

An updated Ecological Appraisal has been provided and has not raised any further concerns. The report is in line with current guidance and is considered appropriate.

With respect to invasive species, whilst there are no invasive species on site, staff should be made aware that there are invasive species present within the site as a whole so they should remain vigilant.

The ecological enhancements detailed in the 'Wold Ecology Outline Condition 30 Compliance Statement' letter are appropriate.

Further information is required in relation to the discharge of Condition 31 of the outline consent relating to the landscape and ecological management plan (LEMP).

### 3.9 Archaeology

#### Original Comments

The reserved matters application includes the latest version of the Archaeological Remains Management Plan (ARMP) created in relation to the National Railway Museum application. As the site falls within the wider York Central boundary the ARMP document sits as an appendix to the main York Central ARMP which is currently undergoing a review and update by TetraTec.

As stated in the document and at pre-application stage an archaeological evaluation is required. Up to 10 trenches have been suggested, but this will depend on potential impacts of the proposed scheme. Ahead of the production of the WSI additional data relating to the York Central site now within the HER will need to be consulted. This includes an updated deposit model report (2020), Phase 1 evaluation assessment and watching brief on GI works 2021.

The evaluation should be taking place in late February/early March and we will need to see an interim report on the findings of the evaluation before commenting on this scheme further.

#### Latest Comments

The first stage of evaluation has been completed, comprising three trenches. Accessible areas were limited due to live services and use of the Museum and Leeman Road. The evaluation did not reveal any significant archaeological features or deposits. Former railway building foundations were revealed at c1m below ground level on the north side of Leeman Road. Further evaluation can be secured by condition. Mitigation will be necessary in the form of a watching brief with excavation where required across the site. This can be covered in the outline conditions and an additional evaluation condition.

### 3.10 Public Protection

Agree with the approach to noise and lighting, however request further conditions to control these matters.

In terms of air quality the submitted statement addresses all required areas and Public Protection considers it acceptable.

3.11 Carbon Reduction Project Officer

The documents provided which relate to Carbon Reduction and Sustainability supply statements of what the development will result in with some reference to how this will be achieved. The report does not commit to anything at this stage and supplies an inference that their admittedly vague energy and carbon targets for the project will be achieved. Conditions to be discharged at a later stage will allow us to request and analyse a more accurate breakdown of the energy data and a breakdown of Carbon Emissions Reduction percentage that will be achieved upon completion of the development.

External

3.12 Holgate Planning Panel

Have objections, we are all concerned about the planned closure of Leeman Road.

3.13 Conservation Areas Advisory Panel

The Panel were concerned at the restricted access for pedestrians through the Central Hall. It would appear however as a 'fait accompli' having already been approved by the City Council. In terms of Central Hall concern was expressed as to how this would function and whether it could cope with large school parties etc and its effectiveness could depend upon adequate signage and visitor management. It was suggested that a railway related feature should be located in either the drum or outside in Museum Square. Development of the former stable block does not form part of the proposals and will be left vacant. Whilst the Panel appreciated the need for a central common entrance, it was felt that the current proposal was missing the 'wow' factor. Perhaps a more prominent glazed structure giving tantalising views of the exhibits would better suit the site.

3.14 Canal and River Trust

No requirement for consultation in this case.

3.15 Environment Agency

No objection in principle to this reserved matters application, on the basis that interests are covered by conditions imposed on the outline planning permission.

3.16 Historic England

Original Comments

The current application is the first proposal to come forward within the sequence of applications across the York Central site. The design of the new Central Hall is well considered and has the potential to make an attractive addition to this

important group of historic buildings and a clear enhancement to the operation, visibility and status of the National Railway Museum.

The application lacks detail and justification to appropriately understand the impact the proposal has on views, to support the demolition of structures such as the goods station mess room; and provides little detail on the landscape proposals. In addition we wish to reiterate the advice given at the outline application with regard to the archaeological potential of the site. It is our view that the evaluation should be carried out pre-determination.

We support the proposals but are concerned by the lack of information in specific areas, particularly archaeology.

#### Latest comments

Welcome the production of a set of images illustrating how the development would be experienced from the City Walls and main Station platforms and the footbridge and have no observations to make.

A Briefing Note and Mess Room Statement has been produced in support of the loss of the mess room however Historic England are still concerned by the loss of the non-designated heritage asset and consider there is room to creatively adapt this building without detriment of the new Central Hall and associated amenity space. Historic England recommend that a holistic approach to the design of Museum Square is adopted which includes the intended uses for ancillary designated buildings i.e. weigh office, bullnose building and the roles they would play as part of the museum offer. This would help more consistently define the proposals for the public realm while ensuring the significance of one of the most important and best preserved examples of a goods station is preserved and if possible enhanced.

Historic England are pleased to see the additional documentation in relation to archaeological evaluation of the site and have no further comments to add.

#### 3.17 Network Rail

No objection in principle to the development.

#### 3.18 North Yorkshire Police Designing Out Crime Officer

It is noted that the application proposes bollard lighting for the northern pedestrian route. Bollard lighting should be avoided as it does not project sufficient light at the right height and distorts the available light due to up lighting making it difficult to recognise facial features and can cause increased fear of crime. It is also susceptible to deliberate or accidental damage. When one takes into consideration that this route is part of the walkway through the museum it will be important to ensure it feels safe to use it is therefore recommended that lighting columns be used instead of bollards. Discussions are ongoing with the Regional Counter Terrorism Security Adviser (CTSA)

regarding a vehicle dynamics assessment which would inform standards required for bollards and security fencing/gates.

3.19 York Civic Trust

No response received.

3.20 Yorkshire Water

Original comments

Yorkshire Water objects to the reserved matters application. Prior to determination the site layout must be amended to account for the public water supply and sewerage infrastructure crossing it.

Latest comments

Without the necessary agreements with Yorkshire Water in place regarding the diversion of public assets, Yorkshire Water carry the risk that the apparatus, both clean and waste will not be moved and therefore affected by the layout of the site. The Yorkshire Water agreements bind the developer financially which is the key factor in approving post agreement.

3.21 Ainsty Internal Drainage Board

The Board recommends a drainage condition be attached.

## 4.0 REPRESENTATIONS

4.1 There have been a total of 98 letters of objection received from local residents, including a letter from the St Peter's Quarter Residents Association Ltd, York Central Action, Friends of Leeman Park, York Cycle Campaign, York Disability Rights Forum, York Green Party and IndieGo Delivery. The comments can be summarised as follows:

Highways

Accessibility through the Museum

- Thousands of residents will no longer have permanently open pedestrian/cycle access across the Railway Museum to the Railway Station and City Centre as the opening times are currently Wednesday to Sunday 10am to 5pm. Residents need access 24 hours a day, 7 days a week, 52 weeks a year.
- Access can be closed up to 10 times a year, excludes dogs, dismantled cyclists, some types of mobility aid and allows personal property to be searched it is therefore not a like for like replacement.
- Proposals run counter to assurances given when outline consent was agreed and need to reflect the intent and public understanding of what was being offered which was free access in perpetuity through the Museum.
- There is no strategy for how residents will be differentiated from museum

visitors etc and no provisions for public use if the Museum has to close for substantial periods.

- More emphasis should be placed on impact on accessibility for residents rather than enhancing the experience of visitors to the Museum.

### Alternative Routes

- Proposal fails to comply with the NPPF which states that priority should be given first to pedestrians and cycle movements both within the scheme and neighbouring areas and address the needs of people with disabilities and reduced mobility in relation to all modes of transport.
- York will soon become home to Active Travel England and has the potential to be an exemplar city for active travel, however these plans fall far short of the ambition York needs to demonstrate.
- It fails to meet the requirement for cycle routes to flow and feel direct and logical and to be accessible to everyone from 8 to 80 and beyond as described in the Government Cycle Infrastructure Design Guide LTN1/20.
- The new road will be three times longer, congested and indirect that will encourage use of motor vehicles rather than walking and cycling.
- The existing riverside walk is not a viable alternative, it is longer, poorly lit, unsafe and often impassable due to flooding.
- Concerned whether the new road will be built before the old one is closed.
- The application is devoid of information on the alternative pedestrian routes and their status (i.e whether they will be waiting to be built etc) and whether they will be safe if they are not overlooked by occupied buildings.
- Distance to bus stops will increase making access to public transport difficult.
- Unclear how the proposals fit with the Council's intentions to develop a Green Infrastructure Strategy for York when it would force thousands of residents to have to use cars, buses or taxis to reach the City Centre.
- Need legal assurance that the riverside route improvements will definitely precede the construction phase of the planning application and suggestions made for how the route could be improved. This should form a condition of approval.
- The Section 106 payment said to have been secured for improvements to the Riverside path is only due on first occupation of dwellings which are likely to be on one of the last parcels to be developed.
- Question why the calculation of travel times used a blanket speed of 5km/hr when less active or able-bodied people are not likely to be able to travel that speed.

### Walkway Agreement

- A walkway agreement that is acceptable to all residents must be secured as it currently severely impacts disabled people, women with children and the elderly.



- There should be an overpass or some sort of cut through as part of the walkway agreement for both pedestrians and cyclists.
- The provision for the new route is through a Walkway Agreement that is not part of the application, despite being referred to several times. The two applications should be designed together along with a Management Strategy showing how the museum will apply the agreement.
- Emphasise comments by the Inspector at the Stopping Up Inquiry that the Walkway Agreement has short comings of significant scale.
- The Walkway Agreement needs to be open to consultation in the same manner as the planning application.
- The Walkway Agreement should include an Equalities Impact Assessment.
- The Walkway Agreement needs to stipulate what the NRM opening hours are otherwise these can change at any time.
- The draft Walkway Agreement tabled at the SUO Public Inquiry stated usual opening hours as 9.00 until 18.00 daily however they have been now cut by 45% from 63 hours to 35 hours per week.

### Design/Layout

- Implore the Council to make the NRM think again and consider an alternative approach to the layout/arrangements for access.
- The idea that the only way to design the NRM requirements can be met via the closure of the road to pedestrians shows a lack of imagination and wilful refusal to consider other options.
- The NRM is already connected underground, why is it that this cannot be expanded.
- The Rijksmuseum in Amsterdam demonstrates a way of how access could be achieved with imagination and good will the NRM can achieve this too.
- There is an opportunity to use the space for social and cultural activity as well as providing a link to the new Museum Square.
- The NRM in its design competition did not ask architects to consider alternative designs to incorporate a public right of way for pedestrians and cyclists.
- The NRM states its intention for the NRM to be carbon neutral, or at least to 'move towards' carbon neutrality by 2033, this is meaningless as any reduction, however small represents a 'move towards'.
- The design of Central Hall is underwhelming.

### Air Quality

- Impacts on air quality given travelling will be longer.
- The proposals also reduce CYCs chances of achieving its target of net zero carbon emissions by 2030.

### Residential Amenity

- There will already be a lot of noise and disruption for years to come, while the site is being built out, shutting of the road to build another building will

add to this.

### Impact on Local Business/Economy

- Money would be better spent elsewhere in the City.
- Stated economic benefits of the development are uncertain and largely unmeasurable.
- Impacts on delivery companies who use cycling to deliver.

### Publicity

- Communication with local residents throughout the entire York Central process has been almost entirely one way, with Applicants informing residents what is going to happen rather than being receptive to change.
- No changes are cited in the Statement of Community Involvement as evidence of concessions made.
- Requests for meetings by organisations such as York Central Action have been ignored by the Applicants.

### General Comments

- The Railway is in a hurry to complete its refurbishment by 2025, this deadline explains why local residents are not being listened to.
- The proposals are going to create an unsafe environment that would increase the likelihood of sexual assaults and muggings.

4.2 A number of residents expressed the fact that they do not object to the expansion of the NRM in principle and value it as a place to visit and understand its contribution to York's Visitor offer, however they remain concerned with the closure of Leeman Road and the proposed access arrangements.

4.3 13 letters of support were received from 2 local residents, Make It York, York Bid, Homes England, York Property Forum, York St John University, York Archaeological Trust, Network Rail, York College, Malmaison, Kevin Hollinrake MP and Julian Sturdy MP which can be summarised as follows:

- Welcome the aspirations of the NRM to reach 1.2 million visitors through its Vision 2025 of which Central Hall is a key element.
- Achieving this target would bring wider economic activity to York City Centre and the wider region to assist the city's post-Covid recovery.
- The NRM's position at the gateway to York Central means it has a wider strategic role.
- The scheme incorporates high quality design with sustainability measures.
- York Central is scheduled to transform this area of the city with 2,500 new homes and a new commercial quarter creating up to 6,500 jobs.
- The NRM can act as the cultural heart of York Central and Central Hall is a positive step towards realising this aspiration as a world class visitor

attraction.

- The NRM's continued success as a truly national museum at the heart of York, coupled with the city's rich railway heritage provides a compelling case for further investment.
- The plans will add to the already fantastic cultural offer in York and help a wider range of other businesses in York to thrive by increasing numbers to the museum and encouraging visitors to stay longer.
- The fact this is the world's largest railway museum means that it brings unique visitors to the city, to see an attraction no other city has.
- Many cities are embracing the UK staycation market and increasing their leisure offerings, therefore York and its attractions need to continue to evolve to compete.
- The proposals will improve the built environment for local residents.
- Proposals are a small inconvenience to motorised vehicles having to use another route and will make the area more pleasant for pedestrians and cyclists, reducing the volume of traffic and pollution for residents in the immediate area.
- Plans for a new Futures Gallery in Central Hall will help to engage young people in Science, Technology, Engineering and Mathematics (STEM) related learning and future careers.
- Proposals are key to supporting York Central as an Enterprise Zone bringing together commercial, educational, cultural and residential use.
- This includes the important steps the NRM are taking towards environmental sustainability in line with the Government mission to achieve Net Zero.
- The new footpaths and bus stops involved in the plans are within walking range for those with mobility issues.

4.4 Cllr Rachel Melly, Cllr David Heaton and Cllr Kalum Taylor as Councillors for Holgate Ward have objected to the proposals and their comments can be summarised as follows:

- Object in the strongest terms to the application.
- Not opposed to principle of NRM extending but oppose the proposals submitted.
- Proposals will have unacceptable significant negative impacts for active travel and parking provision which have not been mitigated, despite it being possible to do so.
- Numerous outline conditions have not been met.
- There is inadequate detail and inaccurate information within the supporting documentation, particularly around bus services.
- The closure of Leeman Road means loss of public access for everyone who uses it for essential access by bus, walking, cycling, mobility aid, or private vehicle.

- Other routes through York Central are longer and more circuitous, subject to daily closures and only available for about a third of the time.
- Even if/when the riverside route is improved it will still be closed due to flooding many times a year and will also feel unsafe for some people.
- Removes the only route between the city centre and Leeman Road residential area which is direct and always available.
- Will disproportionately impact children and disabled people.
- Question claims by NRM that a route over the top of its new building is not possible, allowing unhindered around the clock access for pedestrians and cyclists. There is still an opportunity to include this.
- Condition 45 of the outline permission stipulates arrangements for pedestrians to be able to pass through the proposed new extension.
- A Walkway agreement has been agreed between the NRM and CYC as Highways Authority even though it does the opposite of encouraging sustainable travel.
- Details of the Walkway Agreement are heavily one sided and excessively detrimental to local residents and does not come close to compensating loss of public access.
- There has been no public consultation on the Walkway Agreement, this being central to many residents' concerns.
- The first time the Walkway Agreement was revealed was mid-way through the Public Inquiry into the Stopping Up of Leeman Road and was purely as a supplementary document rather than a matter for decision or amendment.
- Government Inspector for the Public Inquiry published damning feedback on the Walkway Agreement.
- When Committee approved the outline application it was on the basis that *with respect to pedestrians, the in principle acceptance of the closure of a section of Leeman Road is made on the clear proviso that, during the hours of opening of the NRM passage for the public on foot will be freely and directly available in perpetuity, through the NRM from Leeman Road on its North side to Marble Arch*.
- The Walkway Agreement does not provide freely available access, providing limited and heavily caveated access.
- Residents can currently go in a direct route without even having to cross a road.
- If access is available to blue badge holding occupants of the 14 car parking spaces can access, why cannot this be used by residents.
- All the above seeks to discourage people from using the access through the NRM.
- The critical Walkway Agreement between the NRM and CYC which deals with the terms of the permissive route through the new Central Hall is still to be considered as part of this application. The planning consultation cannot be deemed sufficient while this remains the case as it is impossible to fully understand the impact of the plans without being able to scrutinise it.

- The Travel Plan has used inaccurate data about local bus services, which undermines the Applicant's claims linked to sustainable transport and travel plan for staff and visitors. This should be corrected.
- Car parking is based on a multi storey car park that does not exist, is outside the development site and does not even have full planning permission.
- This means the application does not meet the Outline planning conditions for car use reduction and is likely to cause unacceptable parking pressures nearby residential areas.
- The NRM should commit to fund a Residents' Parking Scheme to protect the area.
- Car parking demand has not been based on projected increase in visitor numbers.
- The Outline planning permission stipulates through Condition 38 that car use must be reduced by 30%, the Travel Plan cannot be judged to achieve this due to discrepancies.
- The Outline planning permission required through Condition 52 a strategy for electric vehicle charging facilities, this is absent.
- Serious concerns regarding lighting strategy proposed as lighting appears to be limited. Aside from the dangers this presents it will only discourage active travel.
- The application does not include details to satisfy Condition 30 relating to an increase in biodiversity.
- The ecology survey lacks sufficient detail and relies on future applications to expand biodiversity.
- The BREEAM Report is a long way from the ambition of being an innovative design, highlights energy, pollution and innovation as being factors that are well below the standards of other considered areas.
- Concerns that the noise statement shows a night-time decrease for all areas other than St Paul's Mews, this is a serious concern and needs scrutiny.
- There are inconsistencies in the Heritage Statement.
- Urge the Planning Authority to ensure that the concerns raised by the Internal Drainage Board with respect to discharge to Holgate Beck are responded to.

## 5.0 APPRAISAL

5.1 The key issues for consideration are as follows:

- Context within which to assess this Reserved Matters Application
- Highway matters
- Design, Layout, Appearance and Landscaping
- Heritage Impacts
- Ecology/Biodiversity

- Flooding and Drainage
- Public Protection matters
- Socio Economic matters

## **CONTEXT WITHIN WHICH TO ASSESS THE RESERVED MATTERS APPLICATION**

- 5.2 The outline approval referenced 18/01884/OUTM granted consent for the principle of the redevelopment of York Central to provide a mixed-use development including up to 12,120 m<sup>2</sup> GEA of non-residential institutions (Class D1) for expansion of the National Railway Museum with associated works including new open space, ancillary car parking, demolition of and alterations to existing buildings and associated vehicular, rail, cycle and pedestrian access improvements. As the principle of an expansion to the Railway Museum has been approved this is not a matter for reconsideration as part of the determination of this reserved matters application.
- 5.3 The proposals are to be considered within the context of the Parameters Plans (Condition 6) and Design Guide (Condition 7) approved at outline stage. The approved Parameter Plans cover aspects of the scheme such as the buildings proposed for demolition and the limits of deviation within which new railway additions, access and circulation routes and areas of open space would be developed. It also sets out the different types of development zones across the site and maximum heights and proposed site levels. This application has been brought forward in accordance with the technical documents and parameter plans approved at outline stage. The submission confirms that there are no new significant environmental effects which have been identified and as such any mitigation requirements proposed at outline stage remain unchanged.
- 5.4 Condition 7 of the outline approval requires that development is carried out in accordance with the Design Guide (DG) approved at outline stage. This set out the design qualities of the scheme which the illustrative Masterplan was seeking to achieve and the underlying design intent which future reserved matters applications would need to adhere to. This reserved matters application includes details of how the proposals conform with the approved Parameters Plans and Design Guide which are accepted by Officers.
- 5.5 The outline consent was also subject to a Section 106 agreement and 83 conditions. A number of the conditions imposed relate to site wide aspects, the discharge of these conditions are being progressed by Homes England/Network Rail. However, there are also a series of conditions which relate to the individual phases of development as they come forward. As such there are some conditions which will require formal discharge prior to commencement or at other relevant trigger points within the development

process. Therefore where information has not been presented as part of this reserved matters application each section below confirms which relevant conditions would deal with any outstanding matters. Any new conditions imposed should relate directly to the matters reserved and should not repeat those set out at outline stage as these are still relevant to the development of this site.

## **HIGHWAYS MATTERS**

### Reserved matters to be considered

- 5.6 The highway matters relevant to the assessment of this application should focus on layout and access as set out in Condition 1 of the Outline Planning Permission. Access through the proposed Central Hall is not a reserved matter subject to Condition 1 and this will be dealt with by virtue of the discharge of Condition 45 which will be submitted following reserved matters consent.
- 5.7 The main access to the site for visitors by foot or bicycle would be to the south east through Museum Square. This area will be re-designed as part of the wider York Central scheme and progressed through a future reserved matters application to be delivered by Homes England/Network Rail. The access through Museum Square will provide a level access into the building and it is anticipated that parking for cycles would be provided close to this entrance point once Museum Square is developed. It is not proposed to provide any dedicated car parking spaces for the Museum on this side given that there will be future parking provision through Multi Storey car parks which will also serve the Railway Station. Temporary car parks will also be available across the wider York Central site and the location for these was set out within the reserved matters approval for the infrastructure works.
- 5.8 A secondary access point for pedestrians and cyclists travelling from the north west will also be provided and this access incorporates both a stepped and ramped option to provide level access into the Museum. The access would follow the route of Leeman Road with provision of 14 disabled car parking spaces set within a new landscaped area. CYC Highways have confirmed that the proposed layout of the site and the accesses proposed are acceptable in highway terms and accord with the approved Walkway Agreement, aside from the existing Walkway Agreement Plan which shows a different walkway alignment to that shown on this reserved matters application, the later provides a more direct route along the desire line of the stopped up Leeman Road. As such the Walkway Agreement Plan will need to be varied so that the plan aligns with the proposed reserved matters plan should consent be granted. This does not however prevent reserved matters being granted.
- 5.9 Overall with respect to the layout, positioning and treatment of access and
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circulation routes and how these fit into the surrounding access network Officers are satisfied that the proposals are acceptable. Other matters which are not reserved matters for consideration as part of this application are set out below in order to provide context to Members of Planning Committee.

#### Closure of Leeman Road

- 5.10 When outline consent was granted it clearly set out that Leeman Road would be stopped up to vehicles and cyclists with access through the museum for pedestrians during opening hours. The principle of closing Leeman Road was therefore accepted at outline stage.
- 5.11 As a separate process, under the Highways Act the Applicants applied for a Stopping Up Order (SUO) in order to permanently remove highway rights from a section of Leeman Road. Homes England and Network Rail were granted a Stopping Up Order by the Department for Transport for a 220m section of Leeman Road. This Order came into effect on 6 October 2021 and is subject to a series of stipulations which must be complied with. The SUO enables a section of Leeman Road to be removed as public highway and will come into operation once the Local Highway Authority is satisfied that the conditions of the Order have been met. Prior to the stopping up coming into force, alternative vehicular, pedestrian and cycle routes must be available around the stopped up length of Leeman Road.
- 5.12 There have been many objections received from local residents and interested parties in relation to the closure of Leeman Road however this aspect of the scheme has already been determined in principle through planning as part of the decision to grant outline consent and through highways as part of the Stopping Up Order this is therefore not a matter for reconsideration as part of this reserved matters application.

#### Route through the Museum Site/Walkway Agreement

- 5.13 During the course of the Stopping Up Inquiry the detailed arrangements for the operation of the route through the museum were agreed by virtue of a Walkway Agreement under Section 35 of the Highways Act 1980. The purpose of a Walkway Agreement is to dedicate footways in, through or under parts of a building for use as a footpath. The Walkway Agreement was approved on 27 April 2021 during the Stopping Up Inquiry, with a subsequent Deed of Variation to amend the associated plan dated 11 May 2021.
- 5.14 The Walkway Agreement requires that the reserved matters application for Central Hall shall include the provision of a pedestrian route through the extended NRM site which meets minimum design criteria as to width and gradient (set out in Clauses 3.3.1-3.3.10) and that from the opening date of Central Hall and during opening hours of the museum the walkway shall be



deemed to be dedicated and accepted as a footway in accordance with S.35 of the Highways Act 1980 subject to the conditions set out in the Agreement (Clause 4.1.1) and the permissive path shall be available for use by public on foot as a permissive path subject to the conditions and limitations set out in the Agreement (Clause 4.1.2).

- 5.15 The opening hours of the museum are not stipulated within the Walkway Agreement but are currently Wednesday to Sunday 10am to 5pm outside of school holidays and 7 days during holidays, these are temporary hours. The normal hours of opening are 7 days a week (except Christmas Eve, Christmas Day and Boxing Day) 10.00-18.00 hours between February half term and end of October half term and closing at 17.00 in winter months. Limitations specify that the route can only be used by pedestrians or members of the public travelling in wheelchairs (manual or electric) or by children in prams and pushchairs and there are restrictions as to the improper use of the route (set out at Clauses 4.7.1-4.7.7). Temporary closures are permitted in a limited range of circumstances including things such as royal visits, closures required by law (such as a shutdown of the museum because of a pandemic), for works to be executed and in cases of an emergency or security risk. The Walkway Agreement also provides that, on up to 10 days in any one year, the route can be closed for all or part of the day to enable the NRM to host private functions.
- 5.16 The Walkway Agreement is a formal agreement which has been made under the Highways Act and there will be a legal obligation to keep the route accessible to the public unless it is stopped up in accordance with the provisions and procedure set out in Regulation 6 of the Walkway Regulations 1973.
- 5.17 As part of this reserved matters application Highway Officers have expressed some concern regarding the access arrangements for pedestrians who simply want to pass through the Museum as they are concerned that they may have to join queues alongside visitors which they believe is not within the spirit of what was anticipated at outline stage. The Applicants state that it is anticipated that there would be very limited prospect of pedestrians encountering long queues upon their arrival at the building although this will be monitored and appropriately managed once Central Hall is operational. Furthermore they state that on their approach to the building, pedestrians will be able to identify whether queuing is likely through appropriate signage and will have the option to utilise the alternative routes provided. CYC Highways remain concerned about this aspect of the proposal however Officers note that this is an operational matter beyond the scope of the planning considerations relevant to this application and this matter should have been addressed through the measures set out within the existing Walkway Agreement when it was approved.
- 5.18 A large number of objections received relate to the Walkway Agreement and  
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there has been criticism that the Walkway Agreement was not submitted as part of this application. For clarity the Walkway Agreement has already been approved during the course of the Stopping Up Inquiry and as such is not part of the documents to be determined as part of this reserved matters application. The Applicants make reference to the Walkway Agreement where they consider it necessary to do so and that is adequate for the purposes of assessing this reserved matters application. The Walkway Agreement can be viewed on the Council's website or as part of the Stopping Up Inquiry documents which can be readily found on the internet and it is clear from the comments received that many residents have reviewed the document.

- 5.19 There are also numerous comments which suggest that the design of the walkway route should be amended to provide either an overpass or underpass which enables 24/7 access. Although these comments are noted the Council have to assess the application as presented to them within the context of the outline consent, reserved matters consent for infrastructure and the stopping up decision all of which have already been approved.

#### Alternative route for pedestrians and cyclists

- 5.20 When outline consent and the reserved matters consent for the primary infrastructure works was granted it was recognised that journey times for existing residents walking or cycling would increase by virtue of the closure of Leeman Road. This was accepted on the basis that new more attractive routes would be provided through the York Central site which include segregated lanes for cycles and pedestrians, footpath widths being of modern standard allowing two wheelchairs to pass and being set back from the carriageway with routes being naturally overlooked with enhanced lighting and CCTV.
- 5.21 For pedestrians and cyclists the approved alternative route will provide off road cycle routes and footways on both sides of the new road for most of its length. There will be a new no-through road, footway and cycle-route connecting Leeman Road to the new primary road, referred to as Foundry Way. The alternative route to Leeman Road for pedestrians and cyclists is therefore approved. It was noted by Highways Officers at reserved matters stage that the infrastructure was in line with Cycle Infrastructure Design Guidance LTN 1/20 as far as practically possible. Those areas that were not entirely compliant were due to site/land ownership and other constraints, however these deviations were accepted at reserved matters stage. Details such as surfacing materials, landscaping, lighting, street furniture and CCTV are being dealt with a part of a series of discharge of conditions applications. Homes England/Network Rail, who are delivering the infrastructure works, are currently in discussion with the Council regarding these elements of the scheme. Officers are therefore satisfied that alternative provision for

pedestrians and cyclists has already been secured through earlier consents and planning conditions.

5.22 Some residents have expressed concern as to what the environment around the new alternative pedestrian/cycling route will be. Clearly the York Central site is being developed in phases therefore when the new infrastructure is built out it will, in part, run alongside future building plots. Plots either side of Foundry Way are earmarked for residential use, the route would then sweep around the NRMs South yard which is earmarked for future regeneration as an active public realm including events and play space, the route then runs along Hudson Boulevard to the south of Station Hall where the future plots can be developed for any permitted use at ground floor and a combination of offices and any permitted use at first floor before joining the new road running alongside Museum Square and coal drops area. Discussions are currently ongoing between the NRM, Homes England, Network Rail and North Yorkshire Police concerning CCTV and security matters relating to the infrastructure works to ensure that the site provides a safe environment throughout the build. In addition each reserved matters application will be required to discharge Condition 19 prior to commencement which deals with designing out crime. The Council are therefore satisfied that although the new route may run alongside vacant building plots there are measures in place to ensure that these routes will feel safe for residents to use throughout the build out of York Central.

5.23 It has been recognised through earlier applications that the riverside path is an important alternative route for residents, however this sits outside of the York Central boundary. Concern has been expressed through both the outline and previous reserved matters consent as well as this application that the closure of part of Leeman Road may encourage more residents to use this route as an alternative. The Council note residents' concerns that it regularly floods and that it feels unsafe if used out of daylight hours due to lack of lighting and natural overlooking. Although works to this route do not form part of the York Central scheme and fall outside the scope of this application, the Council in recognising the importance of this route, have recently acquired the land and secured some of the funds necessary in order to carry out improvements to the route for cyclists and pedestrians. The Council aims to have the improvements in place before the stopping up of Leeman Road is implemented, however the programme is dependent on the determination of the final scope of work, availability of funding and relevant approvals including from the Environment Agency.

5.24 As set out above alternative routes for vehicles, buses, pedestrians and cyclists and the impact on travel times have already been assessed in detail during the outline consent, reserved matters consent for infrastructure works and examined by the Inspector during the Stopping Up Inquiry. In all instances the effects of the development on residents through use of

alternative routes has been accepted as a consequence of the York Central development as a whole. Conditions attached to both the outline consent and set out within the Walkway Agreement prevent the stopping up of Leeman Road prior to the new route being provided. This reserved matters application does not alter this situation and Officers are satisfied that alternative provision has been secured through earlier consents and the detail can be secured through relevant conditions to be discharged as part of the outline and reserved matters consents.

#### Alternative route for buses and taxis

- 5.25 The new road infrastructure on the wider York Central scheme will provide an alternative route for both standard bus and Park and Ride services with an increased bus service and there is a commitment in the Section 106 to fund additional services through the site so that there are 4 services per hour in each direction. The Phase 1 infrastructure works includes provision of an eastbound bus lane on Park Street/Cinder Street to give priority for buses and taxis in order to reduce delays. New bus stops, shelters and seating will be provided at intervals along the new highway to serve both existing and new residents. A small bus hub will also be provided adjacent to Museum Square which would serve York Railway Station, the NRM and office workers. All this infrastructure has already been approved and impacts on journey times for residents utilising these services have already been assessed and accepted when both the outline consent and the reserved matters application for the infrastructure was approved.
- 5.26 Concern has been expressed by some residents in respect of the accessibility of bus stops as existing local bus services which currently use Leeman Road will be routed through the York Central site, with a new bus link and stops to be provided on Park Street. The spacing of these stops have been designed as part of the infrastructure RMA so that all residents of York Central will be within easy reach of bus services. The coverage of the Leeman Road Island area will be considered by CYC and the bus operators before they are re-routed, with existing S106 funding used, where required, to ensure adequate coverage. Highway Officers have previously noted that this decision would be made by bus operators in conjunction with CYC teams and is therefore outside the scope of the planning application process. Local Ward Councillors expressed concern that the bus times and numbers were incorrectly quoted within the application. Highway Officers were aware of this at the time of making their comments which were based on the most up to date bus services. The Council are therefore satisfied that bus provision has been adequately addressed through previous consents and there are mechanisms in place going forward to ensure that these measures are implemented.

#### Coach Access

5.27 The infrastructure RMA (20/00710/REMM) made provision for coach access/drop off and pick up for the Museum. Under the infrastructure approval it was proposed that a layby by Museum Square would be provided to enable two coaches servicing the National Railway Museum to set down / pick up passengers. The Applicant advised that European coaches will not be permitted to use these bays as passengers would not be able to alight onto the road carriageway. CYC Highways confirmed under the previous RMA that these arrangements are acceptable in terms of highway impacts and that a Traffic Regulation Order (outside of the planning process) will be required to manage use of the coach bays. This reserved matters application does not alter the proposed arrangements already approved.

### NRM Road Train

5.28 A layby and turning area immediately west of Leeman Road tunnel for drop off / pick up and turning of the National Railway Museum road train were approved as part of the infrastructure works. Under that consent it was envisaged that the road train arriving from the city centre would pull into the layby for passengers to alight and board. The train would then u-turn in the space available to the south of the carriageway and the signal timings of the Leeman Road tunnel would allow the road train to turn out to head back into the city centre while the pedestrian crossing is operational. It was anticipated that the new arrangements for the road train would become operational once the development of Museum Square commences and the road train stop within the National Railway Museum forecourt becomes unavailable. CYC Highways confirmed under the previous consent that these arrangements are acceptable in terms of highway impacts and that a Traffic Regulation Order (outside of the planning process) will be required to manage use of the bays. These proposals would not impact on the arrangements already agreed, although in the interests of highway safety, Highway Officers have recommended a condition be attached to ensure that the arrangements for the road train are agreed.

### Alternative routes for cars

5.29 A number of objections received are concerned with the alternative routes proposed around the site once Leeman Road is stopped up and whilst beyond the scope of this reserved matters application it is useful to set out what those alternative provisions are so that Members are clear on this matter.

5.30 The outline approval for York Central within which the Museum sits was submitted with an Environmental Statement within which highway and transport impacts arising from the development were assessed in a series of detailed Transport Assessments including Traffic Modelling. These were undertaken on the basis that a section of Leeman Road would be stopped up for traffic with a potential pedestrian connection which 'could be through a

building' along the former Leeman Road (Approved Outline Parameter Plan YC-PP-006).

- 5.31 The Transport Assessment accompanying the outline application also set out an analysis of distance and journey times for various modes of transport utilising various route options between existing residential areas to Marble Arch and the Railway Station based on worst case scenarios in respect of traffic numbers. It was therefore recognised and accepted when outline consent was granted that there would be some journey times increased as a result of the NRM expansion and closure of Leeman Road. This was considered in the context that the wider York Central development would comprise high quality and attractive new routes. The impacts on accessibility and increased journey times, particularly for local residents were set out in detail at Paragraphs 16.54 to 16.66 of the OPA Committee Report and accepted by Members at the time of that decision being taken. These anticipated impacts remain unchanged by this reserved matters application.
- 5.32 The first of the York Central reserved matters applications under reference 20/00710/REMM granted in November 2020 sought consent for layout, scale, appearance, landscaping and access for the construction of the primary vehicle route and associated roads, infrastructure, landscaping and alterations to the existing road network pursuant to outline planning permission 18/01884/OUTM. This consent secured the new vehicular, pedestrian and cycle routes through York Central which include alternative routes to Leeman Road.
- 5.33 The approved alternative route for vehicles is through a new length of road linking Garfield Terrace to the west, passing to the south of the NRM before re-joining Leeman Road to the east at the entry to Leeman Road Tunnel/ Marble Arch. The road includes a segregated bus lane over part of its length. The traffic modelling already undertaken assessed the impacts on the existing highway network and indicated that there would be an increase in journey times for vehicles but these were found to be acceptable when granting the reserved matters application for the infrastructure works. These proposals would result in any additional impacts beyond those already identified and approved.

Impact arising from increased visitor numbers on the existing highway network

- 5.34 The OPA Transport Assessment confirmed that the NRM only generates limited traffic in the commuter peak periods and this would not be anticipated to change as a result of the proposed expansion. This was accepted by CYC Highways to be the case when outline approval was granted. In order to provide an updated assessment, this reserved matters application is accompanied by a Transport Assessment which assess impacts arising from the development based on data gathered in respect of visitor and staff trip

rates. The Transport Assessment confirms that there would be a slight increase in peak hour car traffic by 17 cars and 9 cars in the pm peak, this is due to the removal of the free staff car parking, however this traffic is already included within the previous traffic modelling and is below the trigger at which trip making might be noticeable on the highway and as such this small change does not require any further traffic modelling work to be undertaken. CYC Highways accept the contents of the Transport Assessment and raise no objections with respect to highway network impacts.

### Travel Plan/Encouraging Sustainable Transport Modes

- 5.35 The outline planning application was supported by a Framework Travel Plan (FTP) which provided an initial site-wide structure for a proposed 15 year sustainable travel strategy to be implemented. The FTP contained a limited level of detail however it was accepted that this would be developed further at reserved matters stage. Condition 37 of the OPA also sets out that each reserved matters application for a building shall include a development specific Travel Plan to be approved prior to occupation and this must thereafter be adhered to.
- 5.36 The FTP firmly established a quantifiable measure of success in relation to travel plan objectives, namely a principal target which seeks to achieve a minimum 30% reduction in development generated car trips (and a 10% mode split reduction in single occupancy car journeys compared against an agreed baseline position). Funding for the measures proposed in the FTP were secured through the S106 agreement.
- 5.37 The submitted Travel Plan confirms that the plans would generate employment for an additional 35 staff and opportunities for up to 150 volunteers/casual staff spread across the week. A staff travel to work survey undertaken by the NRM in 2021 suggests that the current travel modes by car are high. The proposals will remove free staff car parking so staff would need to shift to alternative modes of travel. It is envisaged that once free car parking is removed for staff travel modes would be 25% walk, 32% train, 18% bus, 5% cycle and 20% car, with those travelling by car parking elsewhere. The NRM propose to include material in staff inductions for travel options available for staff. Staff cycle parking will be provided through 15 covered stands (for 30 bikes), 6 stands (for 12 bikes) adjacent Great Hall with 10 stands (20 bikes) by the conference centre entrance for staff or visitors with staff shower/changing facilities to encourage access to the site by bike. Among other measures, the NRM will also participate in a subsidised cycle purchase scheme aimed to encourage access by cycling. They will also set up a car share scheme within 3 months of site occupation.
- 5.38 With respect to visitors, the submitted Travel Plan states that visitor numbers are expected to increase by an estimated 250,000 per year based on Central

Hall, Wonderlab plus other internal gallery changes. The Travel Plan therefore seeks to promote access to the museum by sustainable transport modes. Travel options to visitors will be displayed on the NRM website. Cycle parking for visitors would largely be provided by a proposed cycle hub planned at the new station entrance with additional visitor spaces provided at both entrances. The existing 20 uncovered cycle parking spaces would be relocated within Museum Square when that phase of the development comes forward. Cycle parking adjacent to the conference entrance will be increased to accommodate 10 stands/20 cycles which will be covered for visitors with a further 21 stands for 42 bikes. A further 11 uncovered cycle stands (22 spaces) will be provided adjacent to the steps by the new walkway close to the Central Hall entrance. A total of 104 cycle parking spaces for staff/visitors will be provided across the site. Within the 14 disabled parking spaces it is proposed to provide 2 electric vehicle charging points. In addition Condition 52 of the outline consent requires that details of electric vehicle charging shall be provided and this will need to be discharged prior to commencement.

### Parking provision

- 5.39 The OPA set out that at any time the car parking provision must not be in excess of ratios prescribed, meaning that any current/temporary car parking would have to be removed concurrent with the opening of new parking, the phasing of car parking is further controlled through Condition 12 of the outline consent. The OPA set out that parking for visitors to the NRM would be provided within a new multi storey car park (MSCP) with 200 spaces provided (a reduction of 134 over existing capacity). It was also intended that 70 spaces would be retained in the NRM north yard for staff. There was concern expressed by Highway Officers at outline stage as to why there was a requirement for 70 spaces as it was felt that this could undermine the ability to achieve sustainable travel plan for the NRM over the long term. It is now the intention that the 70 spaces would no longer be used for staff car parking, other than blue badge or out of hours use which is due to the need to use this area at other times for servicing and storage and occasional low loader access. A further 14 spaces were envisaged at outline stage for disabled badge holders which would be accessed from Leeman Road on the north side and these are included on the layout plan.
- 5.40 It is still the intention of the Museum that car parking provision would be provided at the MSCP, however given that there is no RMA submission for this as yet the Applicants have set out the arrangements for temporary car parking provision until a point that the MSCP is in place. In addition to this the reserved matters application for infrastructure works set out the location of various temporary car parking across the wider site. Condition 12 of the outline approval requires that no development shall commence, other than enabling works of any phase, sub-phase or building and construction of the Primary Vehicle Route (as defined by parameter plan YC-PP 006: Access and



Circulation Routes), until a detailed phasing strategy for car parking has been submitted to and approved in writing by the Local Planning Authority. A part discharge of this condition was granted under reference AOD/20/00109 in order for commencement of the primary vehicle route. It will be a requirement for the Applicant to submit a phasing strategy for car parking in accordance with Condition 12 prior to their development commencing so that the most up to date position can be considered. In addition Condition 48 of the outline consent requires a site specific parking management strategy which will need to be discharged prior to commencement.

- 5.41 Concerns regarding displaced car parking impacting on surrounding residential areas such as the Leeman Road residential area was addressed at outline stage where it was advised that in the unlikely event that car parking was displaced on residential areas this would be mitigated through provisions within the S106 Agreement.

#### Servicing

- 5.42 The Infrastructure RMA stated that a service access would be maintained on Leeman Road to provide access for the NRM, Northern Power and Network Rail to their assets to the north east of the NRM buildings, low loader turning would be available on Foundry Way (the new road adjacent to the NRM south yard), access to the NRM forecourt for inclusive parking, maintenance and servicing would be provided from Leeman Road (west) and Cinder Street. It also stated that Hudson Boulevard is designed to be a service road. The submitted Transport Assessment states the proposals align with the Infrastructure RMA. In addition it clarifies that access to the north service area/car park would be maintained. Condition 49 of the outline consent requires a site specific vehicle servicing strategy to be approved prior to commencement and this will therefore need to be discharged accordingly.

#### Construction Traffic Impacts

- 5.43 It is intended that construction traffic routing will be addressed through the Construction Environmental Management Plan which would need to be discharged through Condition 15 of the outline consent prior to commencement of development.

#### Highway Conclusions

- 5.44 Having had regard to all of the highway related issues CYC Highways confirm that the reserved matters pertaining to the layout and access is acceptable in highway terms and conforms with the approved Walkway Agreement. The proposals are in line with what was accepted at outline stage in terms of traffic generation, impact on the existing highway network, alternative routes for pedestrians and cyclists and parking provision. There are also sufficient

measures in place through conditions and the Section 106 attached at outline stage in order to promote sustainable travel and this is aligned with the Council's transportation policies. The proposals are therefore in accordance with the NPPF and Local Plan Policies set out above. Furthermore the Environmental Compliance Statement confirms that there are no additional effects than were reported in the traffic and transport chapter, and as a result the conclusions of the ES remain valid.

## **HERITAGE IMPACTS**

### Impact on Designated and Non-Designated Heritage Assets

- 5.45 The impacts on heritage assets are assessed in the context of whether the detailed proposals submitted accord with what was set out at outline stage and to establish whether the conclusions of the ES remain valid. In addition applications should be considered in accordance with the Planning (Listed Buildings and Conservation Areas) Act 1990, which states in section 66(1) that local authorities shall have 'special regard to the desirability of preserving the building or its setting' when considering proposals affecting listed buildings or their settings. Section 72 of the same Act requires local planning authorities to pay special attention to the desirability of preserving or enhancing the character or appearance of a conservation area.
- 5.46 Relevant paragraphs of Chapter 16 of the NPPF set out how LPAs should approach determining applications that affect heritage assets. When considering the impact of proposals on designated heritage assets great weight is to be given to the asset's conservation and any harm to or loss of the significance of such assets requires clear and convincing justification. Thus, the provisions of the NPPF import a requirement to identify whether there is any harm to designated and non-designated heritage assets and if so to assess the impact of such harm.
- 5.47 The OPA ES included a Heritage Statement which set out the baseline description of key heritage assets within and around the site. The report acknowledged that the York Central site still contains many railway buildings seen at the turn of the twentieth century valued as part of York's industrial heritage. The buildings were considered to have greater significance when considered as a whole than a set of individual buildings. The most significant of the NRM buildings being the Goods Station, with its unusually intact sequence of Goods Station, Weigh Office and entrance Gateposts all of which are Grade II listed and still associated with a surviving Coal Office, a remnant of the Coal Depot, stables, two LNER traders stores and a mess room which were considered important examples of Victorian processes for handling goods and coal.

- 5.48 The OPA ES stated that the Grade II listed forecourt grouping therefore have a high significance, although the multiple lines of railings and fencing together with the significant levels of parking currently detract from the setting of these buildings. The setting analysis submitted therefore recognised that the proposed York Central development offered positive opportunities for the heritage of the railway land. The OPA identified buildings for retention/demolition and highlighted the former mess room as a building which could be either demolished or retained. Paragraph 4.1.6 of the OPA Heritage Statement states that the mess room is of medium significance due to its aesthetic, evidential and communal value and that conservation would be recommended. There was suggestion at the time that this could be used as a NRM facility.
- 5.49 The submitted reserved matters application proposes the demolition of the current entrance and part of the lean-to building alongside Station Hall, both of which are modern additions attached to listed structures. In addition it is proposed to remove the former mess room a non-designated heritage asset attached to the listed Bullnose building. The submitted Heritage Statement considers that the proposed demolition of modern elements would enhance the opportunity to appreciate the significance of the Goods Shed (Station Hall). In terms of setting the Heritage Statement considers that the proposals would open up views to the Goods Station for visitors to the museum to appreciate. The report considers that Central Hall would enhance the contribution made by the entrance to the setting of nearby listed buildings by introducing attractive new built form. It states that those elements which contribute principally to the significance of the listed buildings at the site (such as the legibility of the railway use) would be unaffected and the ability to appreciate their special interest and their collective interest as an ensemble at the heart of a historic complex would be unchanged.
- 5.50 Historic England commented that few large urban goods station survive nationally, the Goods Station complex in York is considered to be one of the most important and best preserved in the country. It is of special interest for its continued connection to the rail network, degree of intactness and the high survival rate of ancillary structures that were essential to its function. Some of these buildings and structures namely the Goods Station, the Weigh Office, the Coal Manager's Office and House, gate piers and gates are individually listed at Grade II. The survival of other ancillary structures, the remains of the coal depot, former mess room, stables, concrete depot, hydraulic powerhouse, iron foundry and casting shop help to tell the story of how the Goods Station operated and evolved. Historic England consider that together these structures have considerable historic and group value and felt that there was a lack of justification for the demolition of the mess room which contributes to group value of the complex. In addition they felt there was a lack of justification on the impact on the setting of the designated and non-designated assets

resulting from the changes to the way in which the complex would be accessed.

- 5.51 Following a meeting with Historic England the Applicants submitted additional information which explains that whilst the NRM now own the bullnose building they do not own all the land surrounding it. This will be delivered as part of Museum Square and is being delivered by York Central Partnership and as such will come forward as a separate reserved matters application. As set out at outline stage it is intended that Museum Square together with Station Quarter would become the key public space within the York Central site and the public 'front door' to the NRM. It is recognised that this area is important to the setting of heritage assets and the importance of the delivery of this space is noted however this is beyond the control of the NRM and as such should not prevent the determination of this reserved matters application. In referring back to the approved OPA Design Guide it is not difficult to envisage how the bullnose building can successfully be integrated into this area of new public realm. The Applicants state it is not unusual for historic buildings to stand isolated from others and is an approach taken on many regeneration schemes. The Applicants have the intention of improving the bullnose building and bringing it back in to active use which in turn will bring added benefits to the site and its historic setting, however this is currently beyond the scope of this application.
- 5.52 With respect to loss of the non-designated former mess room the Applicants state that its retention was explored through the design process, however demolition was considered the optimal solution in order to provide an appropriate and sympathetic context for the new museum entrance. They consider that the intrinsic architectural and historic interest of the bullnose building would not be affected by the proposals and the loss of the mess room, which is a later addition and thus would be at the lower end of less than substantial harm. They express the benefits deriving from the scheme in terms of the architectural quality of the development proposed, removal of detracting modern additions to better reveal designated heritage assets, enhancing the setting of the listed group of buildings and securing a sustainable future for listed buildings such as the bullnose building. These benefits are in addition to those set out in the planning statement in terms of the economic and cultural benefits the new development will offer the City.
- 5.53 Having re-consulted Historic England remain concerned regarding a cohesive design for Museum Square and public spaces. They note that Condition 23 was attached at outline stage and this required a site wide landscaping strategy to be approved. The Council are in discussion with Homes England/Network Rail in relation to the discharge of this condition. It however remains the case that the NRM do not have control over this land nor are they delivering this reserved matters application. The Council are therefore satisfied that through existing outline conditions and any forthcoming reserved

matters application an appropriate design for this area will come forward in due course and whilst it may have been preferred to have this in delivered alongside Central Hall, this is not the case and it should not prevent the determination of this reserved matters application.

- 5.54 Historic England and the Council's Conservation Officer, whilst not objecting to the scheme remain concerned regarding the loss of the mess room and isolation of the bullnose building. Paragraph 203 of the NPPF states that the effect of an application on the significance of a non-designated heritage asset should be taken into account in determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset.
- 5.55 Officers have taken into account the comments regarding the significance of the non-designated heritage asset and note the information submitted at outline stage and the further Heritage Statements submitted as part of this application. The proposals explain how the retention of this non designated building would lead to a fragmentation of the civic space, reduce its sense of grandeur as set out the OPA Design Guide and would compromise outdoor seating for the Museum by making that area of the site feel secluded and disconnected to the main public square. Whilst the loss of the non-designated mess room is unfortunate Officers are satisfied that the Applicant's have justified their approach. In addition the application clearly sets out the benefits derived from the scheme as referenced above and as articulated in the response from the Council's Economic Development Team. The Council therefore consider that the loss of this non designated heritage asset is outweighed by the significant economic, social and cultural benefits the proposals will bring not only to the City of York but also as a cornerstone of the York Central development.

#### Impact on the setting of the Conservation Area

- 5.56 The station and land to the east of it (including the city walls) lie within the Central Historic Core Conservation Area. Character Area 22 of the Conservation Area relates to the Railway Area which contains a mix of building types, of varying scale and period with many surviving features which relate to the arrival and development of the railway which form a strong narrative when considered alongside the buildings within the York Central site. Many of the surviving buildings within this part of the conservation area are listed and as such have a high significance within a historic setting of high significance.
- 5.57 At outline stage it was recognised that direct impacts on the setting of heritage assets in the Historic Core Conservation Area as a whole were relatively minor. A small number of visual connections would be lost through the

demolition of buildings and development of buildings during later reserved matters phases, however it was considered that these may not necessarily constitute an adverse effect, particularly in terms of the railway heritage of the city. At outline stage it was assumed that several redundant buildings in the railway yards (such as the Bullnose building for example) could be conserved and brought back into use, they could then (through positive design interventions) be reintegrated into York's wider 'railway area' setting. This would benefit the former NER buildings in the Railway Area conservation area, however these would be part of further reserved matters applications. Overall having had regard to the impacts of the proposals on the setting of conservation area it is considered that appropriate consideration has been given to impacts through the design and retention of buildings where possible and that any impacts would be less than substantial and outweighed by public benefits deriving from the scheme.

- 5.58 At outline stage it was established that there would be no impact on St. Paul's Square and Holgate Road Conservation Areas and having considered the layout and design this is still considered to be the case.

#### Impact on setting of Listed Buildings outside the York Central site

- 5.59 The outline application was accompanied by a Heritage Report and Visual Impact Assessment which identified impacts on the setting of and views to and from the city's most renowned buildings, these being the Minster and the City Walls (both of which are Grade I Listed and of very high significance). In addition York Railway Station (Grade II\* Listed), Holgate Windmill, Poppleton Road School and the Fox Inn on Holgate Road (Grade II Listed) were all identified as being of high significance.
- 5.60 The OPA Environmental Statement identified that overall York Central development was not considered to detract from the historic setting of the City as a whole. Page 24 of the OPA Design Guide states that RMAs shall be required to test the scheme against specific townscape views subject to relevance. This is required in order to protect views of York's landmark buildings and structures and the connections between them and the relationship of the historic city to the wider landscape. Whilst it is noted that the proposals would not impact on some of the key views identified at outline stage, Historic England in their initial response requested that further information be provided in respect of long range views of the city's historic core.
- 5.61 The Applicant provided a number of additional key views of the proposed Central Hall from around York Station and the City Walls and Historic England confirmed that they had no further observations or comments to make in respect of these. Similarly the Council's Conservation Officer made no

comment it is therefore considered that there would be no detrimental impact on the setting of Listed Buildings outside of the York Central site.

### Archaeology

- 5.62 The area around York Central has produced significant archaeological remains and the wider site is a complex landscape that has significant potential to preserve locally, regionally, nationally and internationally significant archaeological features and deposits. Despite the York Central site being heavily impacted by the construction of the railway infrastructure in many areas it is believed that there will be pockets of in-situ undisturbed archaeological remains in areas where the natural topography was built-up as part of the 19<sup>th</sup> century levelling works.
- 5.63 The York Central Deposit Model suggests that undisturbed paleo-archaeological and potentially cultural remains may survive throughout the York Central site. The deposit model suggests that these undisturbed deposits may be preserved within the western part of the NRM development area.
- 5.64 Condition 68 attached at outline stage required that as part of any Reserved Matters Application a detailed Archaeological Remains Plan shall be submitted to and approved in writing by the Local Planning Authority. The application is therefore accompanied by an Archaeological Remains Management Plan (ARMP) Version 1.04 dated November 2021. The ARMP conforms with and builds on the principles and requirements set out in the York Central Archaeological Management Plan (Outline Mitigation Strategy) submitted for the wider York Central site.
- 5.65 The Council's Archaeologist advised that the ARMP would sit as an appendix to the main York Central ARMP. The Council's Archaeologist has confirmed that the first stage of evaluation has been completed comprising of three trenches. She advised that accessible areas for evaluation were limited due to live services and the use of the Museum and Leeman Road. The initial evaluation did not reveal any significant archaeological features or deposits. However given the limitations of the evaluation the Council's Archaeologist has agreed with the Applicants that further evaluation works will be undertaken once live services and infrastructure have been decommissioned and therefore a condition is requested in order to secure this.
- 5.66 Historic England have commented that they are satisfied with the evaluation strategy and the evidence presented to date and as such have no further comments. The proposals are therefore acceptable in terms of the approach to archaeology subject to an additional condition. Conditions set out within the outline approval would also need to be adhered to.

## **DESIGN, LAYOUT, APPEARANCE AND LANDSCAPING**

### Design Compliance with Outline Planning Approval

- 5.67 The Design and Access Statement (DAS) submitted at outline stage described the design intent of the development and the key townscape and placemaking considerations. It described how the site would be divided into five distinct areas, each defined by a differing mix of uses and each with its own character, responding to constraints and opportunities and to the design drivers of the development.
- 5.68 The OPA Design Guide advanced the design intent in the DAS and provided guidance for developers in the successful delivery of the development. The Design Guide set out mandatory requirements which subsequent RMAs would adhere to alongside advisory aspirational guidelines which would need to be taken into account by future developers. The Design Guide was conditioned as part of the outline approval (Condition 7) in order to deliver a coherent approved vision in accordance with design guidance as detailed in National Planning Guidance. In addition a series of parameter plans were approved at outline stage (Condition 6).
- 5.69 Each reserved matters application has to be accompanied by a Design Compliance Statement explaining how that phase, accords with the approved Design Guide and Parameter Plans. The application includes a compliance statement which sets out how the proposals accord with the design parameters set out in the OPA as follows:
- The site is within the red line boundary of the outline permission;
  - The assumed maximum gross external area for the NRM extension set out at outline stage was 11,710sqm, the development is within this well within this limit at around 3,500sqm;
  - The limits of deviation for access arrangements for vehicles, pedestrians and cyclists around York Central were set out at outline stage and the primary infrastructure route fixed as part of the reserved matters application;
  - The proposals include 14 disabled car parking spaces for NRM allowed for within the OPA ES;
  - The sequence of development aligns with that set out at outline stage, albeit the construction programme has a delayed commencement; and
  - The scale and mass of the development is within the defined parameters.

### Layout, Appearance and Scale

- 5.70 The outline planning consent showed indicatively the area where a potential extension at the museum site would be located. The proposed extension sits



within this area, however has been reduced significantly in scale and massing which enables a greater relief between the new extension and the listed bullnose building, enables an outdoor seating space onto Museum Square to be incorporated and allows more landscaping to be provided on the northern side of the building.

5.71 Following demolition of the existing entrance and visitor facilities, the sidings (mess room) and various smaller structures around the site it is proposed to construct a largely single storey structure sitting between Station Hall and the Great Hall with a two storey rotunda placed centrally within the structure. The extension would provide new visitor facilities, new gallery space, shop and café and provide level access between the existing gallery spaces. The proposals would unite all the existing NRM buildings. The design and access statement explains that the rotunda has been placed to ensure clear views would be provided to the key museum spaces and would open up views towards the city and allow further appreciation of the site as a former goods station.

5.72 In terms of building heights the design and access statement explains that these are driven in large by the existing structures on site. The maximum height set out within the OPA would only be reached with the central drum which is the key focal point, the eastern wing is much lower to create a transition towards the bullnose building. The western wing responds to the Station Hall with its height set according to the brick detailing on the parapet of Station Hall. Through stepping down the height from Station Hall it indicates the secondary nature and provides a transition towards the scale of the Learning Platform building. These aspects of the scheme were presented to the Council's Conservation Architect within a 3D model and pre-application discussions where it was confirmed that the design response with respect to the scale and height of the proposed building was considered appropriate within its context. The Council's Conservation Architect commented that the proposals are well considered with a strong rationale behind the design development. He considered that the 'rotunda' successfully helps the translation in scale between the existing Great Hall and the much smaller but listed Peter Allen building. It was noted that the setting of the Peter Allen building could have been compromised by the dominance of the Great Hall following demolition to facilitate these proposals. The careful handling of the new design's massing, however, means that the Peter Allen building retains some prominence. Historic England have also commented that the design is well considered, introducing a recognisably contemporary element as the focal point to the assemblage of historic buildings. They consider the simple cylinder of the new Central Hall will serve as a reference point without competing with listed structures and sits well within its context. They consider that the palette of materials is also well chosen. Internally they consider the

proposals are clear and engaging with the introduction of the upper gallery bringing the opportunity to open up views towards the city core and Minster.

- 5.73 Although Museum Square is being delivered by the York Central Partnership as a separate reserved matters application the design and access statement explains that the elevation fronting it has been given careful consideration given that it will be the main focal point. The elevation thus provides a regular window arrangement to the café with some variations with doors providing connection to the outdoor café space. An entrance portico in the middle of the elevation has been aligned with the drum and serves to demarcate the entrance. To the west elevation the appearance is simplified taking into account its adjacency to listed buildings with one key picture window on the elevation. The northern approach is a secondary façade with less intricate brick detailing and features the entrance doors set within the drum. This design approach is supported by the Council's Conservation Architect who was involved in pre-application discussions regarding these elements of the scheme.
- 5.74 In terms of appearance and materials it is proposed to use copper or copper alloy cladding for the proposed drum, this would be treated to prevent the copper transitioning from darker brown to green. At high level the drum would have clerestory glazing exposing the Douglas fir roof structure within. Brick is proposed for facades and metal for the roof coverings. The precise colour of brick has not been chosen at this stage and as such a condition would be necessary in order that sample panels can be inspected prior to commencement. Windows and doors are proposed to be slimline steel or anodised aluminium. It is proposed to incorporate concealed gutters and downpipes. The materials are in line with pre application discussions and again supported by the Council's Conservation Architect subject to conditions that material samples be provided.

### Landscaping

- 5.75 In terms of landscaping as discussed earlier in this report Museum Square would be developed as part of a future reserved matters application therefore the landscaping proposed on this reserved matters relates to that on the western approach. Given that this area of the proposals will provide disabled parking spaces and a servicing entrance the scope for soft landscaping is somewhat limited. Through discussions with the Council's Landscape Architect the Applicants have however maximised the soft landscaping where possible, have introduced tree planting and have sought to reduce the appearance of a truncated road through the use of a mixture of different surfacing materials. The Council's Landscaping Architect is satisfied that the landscaping scheme is appropriate given the constraints of the site. Condition 23 of the outline consent requires a site wide landscaping strategy and this is

currently being discussed with Council Officers and Homes England/Network Rail in order that this condition can be discharged. The Council are however satisfied that the landscaping proposals set out in this reserved matters application will not compromise the site wide landscaping elements currently in discussion. Condition 24 of the outline consent requires site specific landscape details to be approved prior to commencement of development and this condition will require formal discharge aside from this reserved matters application.

### Sustainability measures

- 5.76 The application is accompanied by a BREEAM Pre-Assessment Report. The proposals are for the extension to be sustainably designed with a commitment to low energy, low or non-mechanical systems and alternative energy strategies. This includes using passive design measures, use of low carbon materials such as timber and steel, highly insulated materials to reduce heating demands, mixed mode ventilation provided by mechanical ventilation and heat recovery to relieve overheating and maintaining air quality in summer, air source heat pumps for heating and hot water and sanitary uses, water efficient fittings and meters with leak detection systems, restricted surface water runoffs through below ground attenuation tanks and where feasible permeable paving.
- 5.77 The Council's Carbon Reduction Project Officer has considered the submitted report and states that the report does not commit to anything at this stage and as such they have requested that further conditions be attached. It should however be noted that Condition 51 of the outline consent requires a Design Stage Pre-Assessment Report showing that the building will achieve at least a BREEAM rating of 'Excellent' and this has to be submitted and approved prior to commencement of development with a further requirement for submission of a post development review six months after completion of the development to demonstrate that BREEAM rating of 'Excellent' has been met. The Council are therefore satisfied that there are mechanisms already in place to ensure sustainable construction without further conditions being required.

### Designing Out Crime

- 5.78 With respect to designing out crime the design and access statement confirms it is proposed to address issues of safety through the northern route being designed to be overlooked by museum spaces for most of its length with external lighting to be provided to ensure that the path is well lit and feels secure. The Applicants confirmed that the North Yorkshire Police Designing Out Crime Officer was consulted pre-submission and discussions are ongoing with respect to anti-terrorism measures. The Council are satisfied that designing out crime has been and continues to be considered and it will be addressed in further detail through the future discharge of condition 19 which

relates to secure by design measures being incorporated into the design. This condition will need to be discharged prior to commencement.

### Townscape and Visual Impact

- 5.79 In terms of the townscape and visual impacts arising from the proposals these were considered in the OPA Townscape Visual Impact Assessment (TVIA) which concluded that there would be adverse effects on townscape setting and a number of viewpoints as a result of construction activities, but that the effects were temporary and would vary during the construction programme. These findings were accepted by the Council at outline stage. There are no new or different construction effects than were reported in the OPA TVIA and as a result the conclusions of the ES remain valid.
- 5.80 This reserved matters application sits within the limits of deviation set out at outline stage and proposes a much smaller building than anticipated at outline stage. Some visuals have been provided from the Station and City Walls and it has been confirmed that any impacts on views from Holgate Windmill and Windmill Rise to York Minster and from Bouthwaite Drive to York Minster have been mitigated. The proposals are therefore considered acceptable in respect of their townscape and visual impacts which sit comfortably within what was anticipated at outline stage.

## **ECOLOGY/BIO-DIVERSITY**

### Impact on Habitats and Protect Species

- 5.81 The York Central site as a whole contains extensive areas (9.18ha.) of ephemeral habitat (e.g. the limestone ballast of railway sidings). This is considered to be the most ecologically significant habitat on site due to the invertebrate assemblage it supports, and in part as there are unlikely to be any other sites supporting this extent of habitat elsewhere in York and North Yorkshire. The Ecological Impact Assessment submitted within the OPA ES concluded that the loss of this habitat will result in a permanent moderate adverse effect on ephemeral vegetation and minor adverse effect on scrub and tall ruderal and broadleaved woodland. The ES set out mitigation which would be embedded into the design which comprised of planting 0.43ha of woodland, provision of 0.95ha of ephemeral vegetation, 2180m of green corridor consisting of hedgerow within minimum planting of 80+ trees, creation of 465m of SuDS and 0.4ha wetland waterbody habitat with retained habitats fenced off with a buffer zone if possible sitting alongside a LEMP outlining maintenance post construction. Following this mitigation it was accepted at outline stage that there would be a significant impact on habitat loss arising from the wider York Central scheme.

- 5.82 Whilst this habitat loss was accepted at outline stage, given the extent of this loss it is extremely important that each reserved matters scheme brought forward includes the appropriate mitigation and habitat retention/enhancement set out at outline stage.
- 5.83 A number of ecological surveys on specific species were undertaken at outline stage, however it was recognised that these were to provide baseline information and would need to be updated for each reserved matters phase to reflect changes in the distribution or abundance of mobile species on the site. Condition 28 of the outline consent therefore required that application(s) for reserved matters shall include an up to date (no more than 2 years old) Preliminary Ecological Appraisal (PEA) and any further necessary habitat or species surveys as recommended by the appraisal.
- 5.84 This reserved matters application is accompanied by an Ecological Appraisal dated April 2021 which is valid until October 2022. The survey confirms that there was no evidence of bats or other protected species at the site and no invasive species recorded. The report therefore confirms that there is no further survey work required in these respects. The report sets out a method statement for works which advise of a precautionary approach should there be evidence of bats upon commencement. The report also recommends that at least 2 bat boxes and 2 bird boxes are sited on new buildings on site. The report also advises that a lighting consultant be employed to design lighting with ecology in mind. It also recommends that a detailed Ecological Construction Method Statement and Plan be produced in order to protect, maintain and enhance the site's ecological value.
- 5.85 The Council's Ecologist has confirmed that the report submitted is in line with current guidance and does not raise any further concerns regarding ecological receptors. It is recommended that the measures set out in the appraisal are conditioned. Impacts during construction can be covered by Condition 15 of the outline consent which requires a Construction Environmental Management Plan to be submitted and approved prior to commencement. On the basis of the above it is therefore considered that the proposals accord with the OPA ES in that there have been no significant changes with respect to the habitats or species within the site and as such ecological impacts remain as originally envisaged.

#### Biodiversity Enhancement

- 5.86 With respect to biodiversity enhancement, Condition 30 of the outline approval requires each reserved matters application to provide a Biodiversity Enhancement Management Plan (BEMP) for the creation of new wildlife features to secure net gains for biodiversity. It is recognised that the opportunity for biodiversity enhancement on this part of the site is quite restrained and that greater opportunity for biodiversity enhancement exists

within the South Yard area which will come forward as a future reserved matters application. It is however proposed to provide bird and bat boxes and the Council's Ecologist is satisfied with this approach which aligns with the requirements of Condition 30.

## **FLOOD RISK AND DRAINAGE**

### Flooding

- 5.87 The application site is located within flood zone 1 and as such is at low probability of flooding. Flood risk matters relating to the wider site were addressed as part of the OPA.

### Drainage

- 5.88 At outline stage it was confirmed that separate foul, surface water and highway water drainage systems would be utilised as investigations had shown that infiltration methods of surface water disposal were not suitable. A series of drainage conditions (73 to 82) were attached at outline stage which require formal discharge prior to commencement and discussions are currently ongoing with respect to the discharge of these conditions as part of the infrastructure works therefore any proposals put forward in respect of drainage on this part of the site will need to adhere to the overarching drainage conditions and subsequent details approved.

### Foul Drainage

- 5.89 An existing sewer routes underneath Leeman Road which was identified on the OPA. The OPA set out that the existing Yorkshire Water sewer would be diverted into a new sewer network which would run beneath the new main spine road. Subject to approval from Yorkshire Water it is proposed that the abandoned sewer beneath Leeman Road would undergo a transfer of ownership to become a private drain. The Agent has provided an updated position with respect to the diversion given that Yorkshire Water have raised objections. They state that the contractors for the proposed infrastructure works are currently in dialogue with Yorkshire Water in order to finalise the design for the proposed diversionary routes and complete the Yorkshire Water Agreement. This is a matter which is beyond the control of the Museum however it is noted that Conditions 80 and 81 of the outline planning consent seek to address this matter by requiring the developer to submit evidence to the LPA that the diversion or closure has been agreed with the relevant statutory undertaker prior to construction. The Council are therefore satisfied that this can be appropriately dealt with through the discharge of conditions.

### Surface Water Drainage

- 5.90 The Drainage Report states that the proposals would increase the impermeable areas on site. The Drainage report states that it is proposed to landscape the North Yard footpath with a permeable build up and increase soft landscaping to improve the existing drainage conditions on this part of the site. A cellular soakaway is proposed to dispose of surface water runoff from Central Hall roof and external hardstanding areas. Infiltration testing will therefore be completed to confirm the feasibility of infiltration techniques and if ground conditions exhibit poor infiltration then a cellular attenuation tank is proposed which will limit flows to the public sewer.
- 5.91 The Council' Drainage Engineer has confirmed that there is no objection in principle on the basis that interests are covered by conditions imposed on the outline planning permission.
- 5.92 Yorkshire Water have raised objection to the proposal, although they are aware that a diversion may mitigate this issue for which additional information is required and needs to be signed off by all parties. The Council's Drainage Engineer has stated that comments attached to the outline consent are relevant and once discharged will address the comments raised by Yorkshire Water.
- 5.93 The Environment Agency have raised no objection in principle to the application on the basis that the Environment Agency's interests are covered by conditions imposed on the outline consent.
- 5.94 Having had regard to the consultation responses from the relevant Drainage Bodies, the Council are satisfied that the discharge of planning conditions attached at outline stage can provide the detail required to ensure that an appropriate drainage scheme is incorporated into the site and that there would be no additional impacts in terms of flood risk. The proposals therefore comply with local and national policy with respect to drainage and flood risk subject to discharge of conditions. There are therefore no further impacts beyond those identified within the OPA ES.

## **ENVIRONMENTAL PROTECTION**

### Air Quality

- 5.95 The OPA ES confirmed that taking into account Transport Assessments and Air Quality Monitoring, there would be no residual effects as a result of the York Central development from construction activities subject to implementation of construction dust mitigation measures which would be discharged through Condition 15 (CEMP). In addition the OPA ES established that there would be no predicted residual effects as a result of the development to human or ecological receptors arising from operational traffic

and that any potential impacts arising from temporary car parks would be mitigated by suitable design.

- 5.96 The Environmental Compliance Statement confirms that the cumulative impact of construction in terms of noise disturbance and air quality emissions has not changed and will be controlled through a site specific Construction Environment Management Plan to be discharged through Condition 15. It should be noted that the smaller building footprint/ floorspace will reduce the amount of construction activity and occupancy levels accounted for during the operational phase of development.
- 5.97 Condition 53 was attached to the outline approval and this required that an Emission Mitigation Statement (EMS) be submitted to the Local Planning Authority. This condition was partially discharged by Homes England/Network Rail under application AOD/22/00097. The submitted Emissions Mitigation Statement provides a framework by which all Reserved Matters Applications will be determined through setting out a number of measures for lowering emissions and exposure to air pollution, to deliver the principles of CYC's Low Emission Planning Guidance across the site and over a number of phases of development. The Council's Public Protection Team confirmed that the EMS will need to be adhered to by each reserved matters application with each requiring a statement to cover the specifics of the measures which will be implemented.
- 5.98 The Applicants have provided an Emissions Mitigation Statement as part of this reserved matters application which sets out the specific measures to reduce air emissions which will be employed. These measures include promoting active travel to reduce vehicle emissions, electric vehicle charging provision, reduction in building emissions, achieving BREEAM excellent through high levels of insulation, efficient building fabric and ventilation, servicing utilising modern passive design principles and installation of air source heat pumps. The Council's Public Protection Team have confirmed that the Emissions Statement is acceptable in accordance with the requirements of Condition 53. The proposals do not give rise to any additional impacts beyond those set out the OPA Environmental Statement.

### Noise and Vibration

- 5.99 The Planning Statement submitted confirms that the cumulative impact of construction in terms of noise has not changed and will be controlled through a site specific Construction Environment Management Plan to be discharged through Condition 15. Outline condition 64 relates to details of plant and machinery for non-residential premises and this will require formal discharge prior to installation. The application includes a Noise Control Strategy for plant noise which establishes upper limits at sensitive locations within the museum site. The noise generating equipment to be installed include air source heat



pumps and any required ventilation equipment. Plant noise emission upper limits have been set at sensitive locations within the museum site. These include sensitive museum facades (with opening windows) and areas that may potentially be used as public external amenity space. Identified receptors to noise are located in excess of 150m from Central Hall and by designing new plant equipment to meet upper limits within the museum site the limits set out within the OPA ES would be comfortably be met.

5.100 The Council's Public Protection Officer has requested that a condition be attached relating to noise, however these matters are already covered by Condition 64 which would need to be discharged accordingly.

### Contamination

5.101 No additional contaminated land information has been submitted as part of this RMA application as appropriate contaminated land assessments were submitted and approved as part of the OPA. In addition Condition 55 was attached at outline stage which requires that prior to commencement of each development phase or sub phase a site specific investigation and risk assessment needs to be undertaken to better understand the contamination on site. This condition will therefore need to be discharged by the Applicants accordingly. This assessment will then inform Condition 56 which relates to a remediation scheme being submitted. In addition any sub soil or top soil materials being imported to the site will be analysed to ensure it is suitable for the intended use, which will be agreed with the LPA to satisfy condition 59.

5.102 Following the implementation of mitigation there will be no new or different construction effects than were reported in the OPA ES and as a result the conclusions remain valid. The Council's Public Protection Officer have confirmed that they have no objections as the outline planning conditions adequately deal with this matter.

### Light Pollution

5.103 Condition 22 of the OPA requires that a lighting strategy be submitted with any reserved matters application. A Lighting Report has been submitted which sets out the anticipated approach to both internal and external lighting. The Council's Public Protection Team have confirmed that the strategy is accepted however have recommended that a condition be attached to ensure that a lighting spillage plan is provided to demonstrate the lighting levels at the nearest residential properties to demonstrate that they will not be adversely affected by lighting from the development.

5.104 With respect to matters of environmental protection, the proposals are considered to be in accordance with the OPA ES which accepted any impacts subject to mitigation and a series of conditions to be discharged. The

proposals are therefore in accordance with the OPA ES and accord with national and local policies.

## **SOCIO ECONOMIC IMPACTS**

- 5.105 The proposals are projected to increase footfall to the museum to 1 million visitors per year. The Council's Economic Development Team have noted that the increase in footfall to 1 million visitors per year, will mean visitors visiting the City for one day, many for longer, boosting the local economy particularly the hospitality and cultural sectors. Inevitably the increase in building size and variety of specialist areas will also create new jobs as well as securing the long-term future for people already employed by the Museum.
- 5.106 The Economic Development Team note the educational benefits of the museum which actively encourages interest in STEM subjects and the proposed Wonderlab will allow children aged between 7 and 14 to participate in engineering workshops, helping to nurture future generations of talent, some of whom will be of key benefit to burgeoning sectors already established in York. They also recognise that the NRM is integral to the development of York Central, complementing and enhancing the unique qualities of the project. Therefore given the economic significance provided by the proposed expansion the Council's Economic Team support this application.
- 5.107 As part of the consultation for the application a number of other organisations within the City have expressed their support for the scheme in terms of the economic and education benefits it will provide.
- 5.108 The proposals are in line with the socio-economic impacts set out at outline stage and accord with the policies set out above.

## **6.0 CONCLUSION**

- 6.1 The principle of development of the NRM site as part of the wider York Central development was approved at outline stage and the proposals put forward within this reserved matters application are within the remits of the approved parameter plans and design guide approved by Conditions 6 and 7.
- 6.2 The outline application was granted in the context that Leeman Road would be stopped up and alternative routes provided through the York Central site. The Stopping Up of Leeman Road has been granted through a separate highway process. As part of the Stopping Up a Walkway Agreement was approved which set out operational matters with respect to access through the museum. This reserved matters application seeks approval for access and layout and the Council are satisfied that the proposals provide an appropriate layout and access to the site and accord with the Walkway Agreement.

- 6.3 The proposals are in line with what was accepted at outline stage in terms of traffic generation, impact on the existing highway network, alternative routes for pedestrians and cyclists and parking provision. There are also sufficient measures in place through conditions and the Section 106 attached at outline stage in order to promote sustainable travel and this is aligned with the Council's transportation policies. The proposals are therefore in accordance with the NPPF and Local Plan Policies set out above.
- 6.4 With respect to heritage impacts, the Council are satisfied that the proposals would not result in harm to designated heritage assets on the site. It is recognised that delivery of Museum Square would have provided more certainty with respect to the setting of heritage assets however this is not within the control of the NRM and the Council are satisfied that this will be adequately addressed through a future reserved matters scheme for this site. It is recognised that there will be loss of a non-designated heritage asset, however the Applicants have justified their approach to the design and loss of the mess room. In addition the application clearly sets out the significant economic, social and cultural benefits derived from the scheme. The Council therefore consider that the loss of this non designated heritage asset is outweighed by the significant benefits the proposals will bring not only to the City of York but also as a cornerstone of the York Central development. With respect to archaeology appropriate investigations have been undertaken as far as they can at this stage of the development and it is agreed that further investigations will be required once more of the site becomes accessible. The Council and Historic England are therefore satisfied that an appropriate approach to archaeology is being taken.
- 6.5 The proposals provide a satisfactory layout, appearance, scale and landscaping scheme which accord with the outline Design Guide and would enhance the character and appearance of this area. Whilst sustainability and designing out crime measures have been set out, the full details of these measures will be secured through subsequent discharge of conditions.
- 6.6 The application includes an appropriate update in terms of impacts on habitats and protected species within the site which remain in line with the OPA ES.
- 6.7 The Council are satisfied that the discharge of planning conditions attached at outline stage can provide the detail required to ensure that an appropriate drainage scheme is incorporated into the site and that there would be no additional impacts in terms of flood risk.
- 6.8 The proposals are considered to be in accordance with the OPA ES which accepted impacts with respect to air quality, noise and contamination subject to mitigation and a series of conditions to be discharged.
- 6.9 The economic benefits arising from the scheme are recognised and the

contribution the proposals will make to the City are supported by the Council's Economic Development Team.

The application is therefore recommended for approval subject to the following conditions:

1. The development hereby permitted shall be carried out in accordance with the following plans:-

Location Plan NRM-FFA-ZZ-00-DR-A-01001 P2-3

Proposed Site Layout (Walkway Agreement) 201564\_NRM\_OP\_SW\_0001 Rev 05

Proposed Ground Floor Plan NRM-FFA-CH-00-DR-A-01100 P2-3

Proposed First Floor Plan NRM-FFA-CH-01-DR-A-01101 P2-3

Proposed Roof Plan NRM-FFA-CH-RF-DR-A-01102 P2-2

Proposed Museum Square Elevation NRM-FFA-CH-XX-DR-A-02101 P1-1

Proposed North Approach Elevation NRM-FFA-CH-XX-DR-A-03011 P1-1

Futures Gallery Bay Study NRM-FFA-CH-XX-DR-A-05102 P1-1

Café Bay Study NRM-FFA-CH-XX-DR-A-05101 P1-1

Proposed Long section NRM-FFA-CH-XX-DR-A-03103 P1-1

Proposed Short Section NRM-FFA-CH-XX-DR-A-03102 P1-1

Proposed Futures Gallery Section NRM-FFA-CH-XX-DR-A-03100 P2-2

Pedestrian Route Long Section SCP/210061/SK04 Rev B

Pedestrian Route Chainage SCP/210061/SK03 Rev B (Approved in so far as it relates to site levels only)

Proposed General Arrangement Soft Landscape  
201564\_NRM\_OP\_SW\_0003 Rev 04

Proposed General Arrangement Hard Landscape  
201564\_NRM\_OP\_SW\_0002 Rev 04

Demolition Plan NRM-FFA-CH-00-DR-A-00500 P1-1

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

2. Prior to commencement of construction above ground level details and/or samples of all external wall and roofing materials shall be submitted to and approved in writing by the Local Planning Authority. The development shall thereafter be carried out using the approved materials. For external walling this shall include a 1m square sample panel of the brickwork to be used on the buildings to be erected on the site which shall illustrate the colour, texture and bonding of brickwork and the mortar treatment to be used. The development shall be constructed in accordance with the approved sample panel(s) which shall be retained through the period of construction of the approved development.

Reason: In the interests of good design and visual amenity, in accordance

with the NPPF.

3. Prior to commencement of construction above ground level the following details shall be submitted:

- 1:5 vertical cross section through the junction between the rotunda roof and the glazed walling, and junction of the glazing with timber panels;
- 1:5 horizontal section through the rotunda glazing and frames, and the junction of solid panels beneath, illustrating part panel and joints.
- 1:5 Vertical cross sections through the frontage elevations illustrating inter alia roof junction detail, walling and set back of window and door openings.

The details shall thereafter be approved in writing by the Local Planning Authority and shall be carried out in complete accordance with the approved details.

Reason: In the interests of good design and visual amenity, in accordance with the NPPF.

4. Proposed details for the making good of the wall of the bullnose building following removal of the former mess room building needs to be submitted and approved in writing by the Local Planning Authority and thereafter be carried out in complete accordance with the approved details.

Reason: In the interests of good design and visual amenity, in accordance with the NPPF.

5. The approved general arrangement drawing for soft landscape referenced 201564\_NRM\_OP\_SW\_0003 Rev 04 shall be implemented within a period of six months of the practical completion of the development or the earliest available planting season. Any trees or plants which within the lifetime of the development die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of a similar size and species, unless the Local Planning Authority agrees alternatives in writing.

Reason: The landscape scheme is integral to the amenity of the development.

6. The hard landscape works shall be carried out in complete accordance with the approved general arrangement drawing for hard landscape 201564\_NRM\_OP\_SW\_0002 Rev 04. Within three months of commencement of development a product specification and details of the following shall be submitted to and approved in writing by the Local Planning Authority:

Street furniture, gates, walls, fencing, edging, and surfacing, including colour, unit size, paving pattern/coursing, and sample materials.

Reason: The hard landscape scheme is integral to amenity and functionality of the development and outdoor space.

7. The archaeological scheme comprises 3-5 stages of work. Each stage shall be completed and agreed by the Local Planning Authority before it can be approved.
  - A) No archaeological evaluation or development shall take place until a written scheme of investigation (WSI) for evaluation and a watching brief across the site has been submitted to and approved by the Local Planning Authority (LPA) in writing. The WSI should confirm to standards set by the LPA and Chartered Institute for Archaeologists.
  - B) The site investigation and post investigation assessment shall be completed in accordance with the programme set out in the Written Scheme of Investigation approved under Condition (A) and the provision made for analysis, publication and dissemination of results and archive deposition will be secured. This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI.
  - C) A copy of a report on the evaluation and an assessment of the impact of the proposed development on any of the archaeological remains identified in the evaluation shall be deposited with City of York Historic Environment Record to allow public dissemination of results within 6 weeks of completion or such other period as may be agreed in writing with the Local Planning Authority.
  - D) Where archaeological features and deposits are identified proposals for the preservation in-situ, or for the investigation, recording and recovery of archaeological remains and the publishing of findings shall be submitted as an amendment to the original WSI. It should be understood that there shall be a presumption in favour of preservation in-situ wherever feasible.
  - E) No development (other than demolition or any enabling works that do not relate to archaeology) shall take place until:
    - Details have been approved and implemented on site;
    - Provision has been made for analysis, dissemination of results and archive deposition has been secured;
    - A copy of a report on the archaeological works described in Part D should be deposited with City of York Historic Environment Record within 3 months of completion or such other period as may be agreed in writing with the Local Planning Authority.

This condition is imposed in accordance with Section 16 of the NPPF.

Reason: The site lies with an Area of Archaeological Importance. Further investigation is required to identify the presence and significance of archaeological features and deposits and to ensure that archaeological features and deposits are either recorded, or if of national importance, preserved in-situ.

8. Prior to the buildings being brought into use, a signage strategy which shall include the design and position of notification signs for pedestrians of when access through the museum is closed to both the east and western approach to the site shall be submitted to and approved in writing. The signage shall thereafter be implemented in accordance with the approved strategy.

Reason: To ensure that pedestrians are clear as to when to use alternative routes to save abortive walking distances for non-visitors to the museum.

9. Prior to the commencement of construction above ground level details of the cycle parking areas, including means of enclosure shown on Drawing 201564\_NRM\_OP\_SW\_0001-REV05, shall be submitted to and approved in writing by the Local Planning Authority. The building shall not be brought into use until the cycle parking areas and means of enclosure have been provided within the site in accordance with such approved details, and these areas shall not be used for any purpose other than the parking of cycles.

Reason: To promote use of cycles by both staff and visitors in the interests of sustainable travel.

10. Once the building is brought into use, the NRM road train will not operate other than in accordance with the access arrangements approved in reserved matters application 20/00710/REMM unless alternative access arrangements have been submitted to and approved in writing by the Local Planning Authority

Reason: In the interests of highway safety.

11. Details of the height, type, position, angle and spread of any external lighting shall be submitted to and approved in writing by the Local Planning Authority prior to the development hereby permitted being brought into use. The external lighting shall be erected and maintained in accordance with the approved details to minimise light spillage and glare outside the designated area.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area and in the interests of designing out crime and ecology

so as to ensure that lighting will not be mounted where it would directly impact on bat boxes, bird boxes or surrounding tree cover.

12. The proposed development shall be undertaken in accordance with the biodiversity gains and recommendations set out from Paragraph 8.4.7 of the submitted Ecological Appraisal dated April 2021 by Wold Ecology. This includes:

- Tree removal being carried out outside of bird nesting season;
- 3 Schwegler swift boxes to be installed;
- 3 Schwegler sparrow boxes to be installed;
- The Construction Management Plan to be discharged under Condition 15 including considerations on ecology and wildlife and how these will be protected throughout the build process.

The proposed boxes to be installed prior to the building being brought into use.

Reason: In order to ensure that habitats and species are appropriately protected throughout the development.

## **7.0 INFORMATIVES:**

### **STATEMENT OF THE COUNCIL'S POSITIVE AND PROACTIVE APPROACH**

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome:

Worked with the Applicant during the course of the application to seek clarity and amendments where necessary in order to ensure that the overall layout and design was satisfactory and accords with the design parameters established at outline stage.

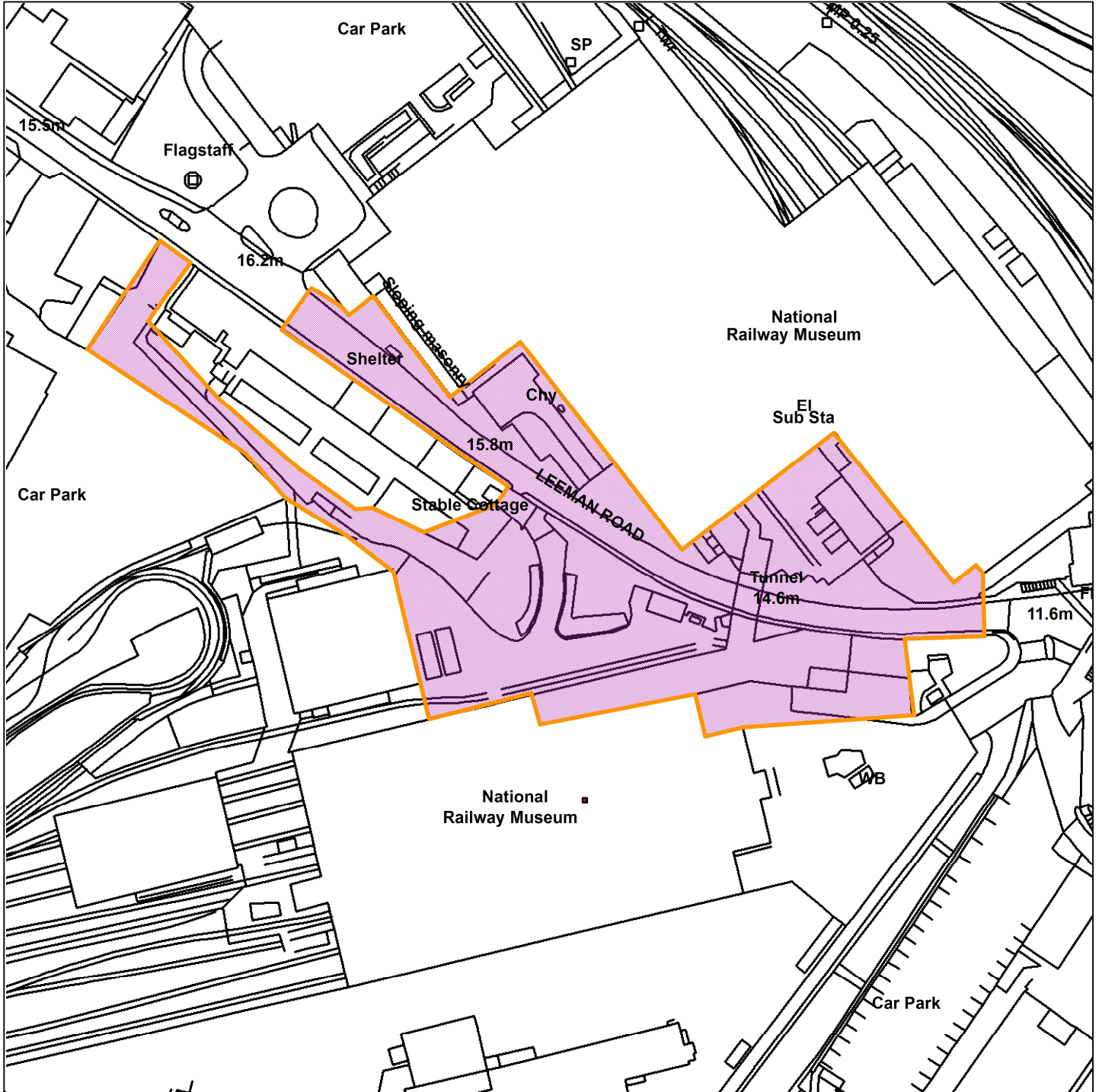
#### **Contact details:**

**Case Officer:** Louise Milnes  
**Tel No:** 01904 555199



21/02793/REMM

Railway Museum Annexe, Leeman Road



Scale : 1:1526

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<b>Organisation</b>	City of York Council
<b>Department</b>	Directorate of Place
<b>Comments</b>	Site Plan
<b>Date</b>	08 June 2022
<b>SLA Number</b>	Not Set

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**City of York Council**  
**Equalities Impact Assessment**

**Who is submitting the proposal?**

<b>Directorate:</b>	Place
<b>Service Area:</b>	Planning and Transport
<b>Name of the proposal :</b>	Planning application 21/02793/REMM - Reserved matters application for layout, scale, appearance, landscaping and access for the construction of Central Hall (F1 use class) including entrance hall, exhibition space and café with associated access, parking, landscaping and external works following the demolition of the mess room and other structures pursuant to 18/01884/OUTM
<b>Lead officer:</b>	The planning application was submitted by the National Railway Museum which is part of the Science Museum Group. CYC officers:

	<ul style="list-style-type: none"><li>- Development Management Officer: Louise Milnes</li><li>- Highway Development Control officer: Helene Vergereau</li></ul>
<b>Date assessment completed:</b>	22 July 2022

<b>Names of those who contributed to the assessment :</b>			
<b>Name</b>	<b>Job title</b>	<b>Organisation</b>	<b>Area of expertise</b>
Helene Vergereau	Traffic and Highway Development Manager	CYC	Highways and Transport
Tony Clarke	York Central Highway Authority Lead	CYC	Highways and Transport
Louise Milnes	Development Management Officer	CYC	Planning
Becky Eades	Head of Planning and Development Services	CYC	Planning
James Gilchrist	Director of Environment, Transport and Planning	CYC	Highways and Transport
Heidi Lehane	Senior Solicitor Planning	CYC	Legal

## **Step 1 – Aims and intended outcomes**

<b>1.1</b>	<b>What is the purpose of the proposal?</b> Please explain your proposal in Plain English avoiding acronyms and jargon.
	This reserved matters application is for the development of Central Hall, an extension to the National Railway Museum to be located across Leeman Road, following the closure of the road. The new building will connect the museum's two main galleries at surface level. It will also provide a route for pedestrians through the Museum during opening hours, linking Leeman Road/Cinder Street to the south/east of the Museum to Leeman Road and the residential areas to the north/west of the Museum.

<b>1.2</b>	<b>Are there any external considerations?</b> (Legislation/government directive/codes of practice etc.)
	<p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impacts of these decisions are therefore not assessed here.</p> <p>The established planning consents are:</p> <ul style="list-style-type: none"> <li>- 18/01884/OUTM for the redevelopment of York Central to provide a mixed-use development of up to 379,729 m2 of floorspace Gross External Area (GEA) primarily comprising up to 2,500 homes (Class C3), between 70,000 m2 and 87,693 m2 of office use (Class B1a), up to 11,991 m2 GEA of retail and leisure uses (Classes A1-A5 or D2), hotel with up to 400 bedrooms (Class C1), up to 12,120 m2 GEA of non-residential institutions (Class D1) for expansion of the National Railway Museum, multi-storey car parks and provision of community uses all with associated works including new open space, ancillary car parking, demolition of and alterations to existing buildings and associated vehicular, rail, cycle and pedestrian access improvements.</li> </ul> <p>18/01884/OUTM includes the principle of the closure of Leeman Road, the provision of alternative routes around the Museum through Cinder Lane (all users) and Hudson Boulevard (for pedestrians and cyclists), the provision of a pedestrian access through the National Railway Museum extension during hours of opening (Condition 45), and the upgrade of the riverside path between Scarborough Bridge and Jubilee Terrace (S106 funding).</p> <ul style="list-style-type: none"> <li>- 20/00710/REMM for layout, scale, appearance, landscaping and access for the construction of the primary vehicle route and associated roads, infrastructure, landscaping and alterations to the existing road network pursuant to outline planning permission 18/01884/OUTM</li> </ul> <p>20/00710/REMM includes the layout of Leeman Road and Cinder Lane, including the pedestrian and cycle routes and facilities on Leeman Road to the south of the Museum, Cinder Lane and Hudson Boulevard.</p>

	<p>The Leeman Road stopping up order is dated 23 September 2021 and includes the Secretary of State decision and associated Inspector's report</p> <p>Legislation, Government guidance, standards</p> <ul style="list-style-type: none"> <li>- Highways Act 1980, Town and Country Planning Act 1990, Equality Act 2010, Human Rights Act 1998</li> <li>- Inclusive Mobility, A Guide to Best Practice on Access to Pedestrian and Transport Infrastructure</li> <li>- BS 8300-2:2018 Design of an accessible and inclusive built environment. Buildings - code of practice</li> </ul>
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<b>1.3</b>	<b>Who are the stakeholders and what are their interests?</b>
	<p>National Railway Museum (NRM) visitors</p> <p>People passing through the NRM, for example: local residents and people travelling to/from the Leeman Road area to the north/west of the Museum.</p> <p>Future users of the York Central development</p> <p>Specifically, within these user groups: people with reduced mobility, people using a wheelchair or mobility aid, people with sensory impairments, people with pushchairs or with young children and people with other protected characteristics</p> <p>Cyclists including users of adapted cycles</p>



<p><b>1.4</b></p>	<p><b>What results/outcomes do we want to achieve and for whom?</b> This section should explain what outcomes you want to achieve for service users, staff and/or the wider community. Demonstrate how the proposal links to the Council Plan (2019- 2023) and other corporate strategies and plans.</p>
	<p>The objectives are set out in the planning application documents for Central Hall (21/02793/REMM) and form part of the wider strategy for the York Central development site approved under 18/01884/OUTM.</p> <p>In relation to the closure of Leeman Road, paragraph 16.65 of the Committee Report for outline application 18/01884/OUTM confirms that:</p> <p><i>With respect to pedestrians, the acceptance (in principle) of the closure and requirement for a subsequent stopping up order, is made on the clear proviso that, during the hours of opening of the NRM, passage for the public on foot will be freely and directly available in perpetuity, through the NRM (buildings/land) from Leeman Road on its north side to Marble Arch</i></p> <p>This is secured by outline planning condition 45, which states that</p> <p><i>Prior to the closure of Leeman Road for pedestrians and cyclists a scheme for a new alternative route for pedestrians and cyclists and details of a pedestrian access through the National Railway Museum extension shall be submitted to and approved in writing by the Local Planning Authority. The approved alternative new access for pedestrian and cyclists shall be implemented before Leeman Road is closed for pedestrians and cyclists. The pedestrian access through the National Railway Museum shall be implemented on the opening of the extension and made available during hours of opening. 18/01884/OUTM Page 20 of 35</i></p> <p>The NRM’s Vision Statement submitted as part of the reserved matters application explains the museum’s aspirations for Central Hall, stating that:</p> <p><i>“Central Hall is not just about a better arrival and visitor experience for the museum. It has a fundamental role as the cultural heart of the York Central development.”</i></p>

	<ul style="list-style-type: none"> <li>- <i>“The key gallery space of the current estate, the vast Great Hall will continue to present and interpret the world the railways made, but in a more engaging and immersive way so that its central story and showpiece collection items are truly brought to life”.</i></li> <li>- <i>“Central Hall will unite the core museum buildings at ground level combining a new entrance experience with public-facing spaces for exhibition, orientation and reception, eating, museum shop and events. The building will remove the need for the existing underpass which restricts free access between the exhibition halls and in its place will provide seamless level access between the existing exhibition halls, creating a unified integrated museum”.</i></li> <li>- <i>“The key aims of Central Hall are to create a new public face to the museum and to provide an appropriate forecourt to, and entrance sequel from, York Railway Station and beyond. The ambition is to provide a compelling and appealing public-facing welcome and arrival space for orientation and reception. Central hall will be very much the catalyst that connects. Rationalises and integrates the museum estate”.</i></li> </ul>
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## Step 2 – Gathering the information and feedback

<b>2.1</b>	<b>What sources of data, evidence and consultation feedback do we have to help us understand the impact of the proposal on equality rights and human rights?</b> Please consider a range of sources, including consultation exercises, surveys, feedback from staff, stakeholders, participants, research reports, the views of equality groups, as well your own experience of working in this area etc.	
	<b>Source of data/supporting evidence</b>	<b>Reason for using</b>
	Representations received in response to the NRM planning application (available	Members of the public were able to view the planning application proposals and comment on them during the consultation period. Some of

on the [CYC planning portal](#) under planning reference 21/02793/REMM)

the representations received identify issues which are relevant to this assessment. These can be summarised as follows:

- Issues considered as part of the NRM application
  - Access to the first floor gallery for wheelchair users - a passenger lift is provided to access the first floor gallery, which is suitable for wheelchair users and pushchairs (double) i.e. through lift with space to turn
  - Navigating Central Hall, particularly for less able pedestrians
  - Safety for lone women travelling along the walkway route
  - Queuing and delays entering the building for disabled users and the elderly
  - Increased journey lengths along the Walkway Route for the less able
  - Use of mobility scooters
  - Access through the museum for disabled cyclists / hand bikes
  - Level access
  - Lighting along the walkway route
- Issues outside the scope of this assessment
  - Alternative routes – comments on the primary vehicle route, riverside path and pedestrian / cycle links all introducing longer travel times and presenting safety issues with a lack of natural / passive surveillance. These issues are compounded for disabled users, women and the elderly
  - Blue badge spaces - the number of blue badge spaces (14) was considered as part of the outline planning application and the related traffic modelling. The reserved matters application is pursuant to the outline consent

	<ul style="list-style-type: none"> <li>○ The availability of the route during opening hours impacts more significantly on vulnerable groups - Outline planning condition 45 specifies that the route should be available during opening hours and the reserved matters application has been brought forward in line with that requirement.</li> <li>○ Temporary closures of walkway route for events – the number of closures is managed through the Walkway Agreement</li> <li>○ Location of bus stops - The location of bus stops along the primary vehicle route is defined by the reserved matters approval for the proposed infrastructure works under reference 20/00710/REMM</li> </ul>
<p>Documents submitted with the planning application 21/02793/REMM (e.g. Transport Assessment and plans submitted)</p> <p>Additional document submitted by the NRM for this assessment “Central Hall Walkway Route – Signpost to the relevant accessibility features of application 21/02793/REMM”</p>	<p>These documents provide information on the proposed route to/from and through the museum, expected flows, times of operation, design of the route, etc</p>
<p>Leeman Road stopping up Inspector report</p>	<p>The Inspector’s report for the Leeman Road stopping up order summarised the issues linked to the stopping up of Leeman Road within paragraphs 9.97 and 9.98 of his report:</p> <p><i>“With the mitigation proposed as part of the Phase 1 Infrastructure works in place, no one would be unable to make any of the journeys that they currently make via Leeman Road. For vehicle users, journey distances would not be significantly greater and users would benefit from being able</i></p>

	<p><i>to use the high quality highway infrastructure proposed. There would, however, be a financial penalty on taxi users and, potentially, on taxi and private hire drivers, at least over the short term, if this led to a reduction in taxi use by existing residents in the area. Cyclists would experience slightly longer journey times but would be able to use purpose-designed infrastructure which would make their journeys safer and which would be likely to encourage increased use of cycles.</i></p> <p><i>The pedestrian route through Central Hall would provide limited mitigation for the loss of Leeman Road as a direct pedestrian route to and from the City Centre and Railway Station. Although not as direct, the alternative route via Foundry Way and Hudson Boulevard would provide a suitable alternative for most pedestrian journeys. This would involve an increase in travel distance and journey time but, in the long term, those disbenefits would be offset by the ability to use a new high quality route, with landscaping and resting places, that passes through and gives access to the new facilities and amenities to be developed as part of the YC scheme”</i></p>
<p>Census 2011 data</p>	<p>Review of data for the two Lower Super Output Areas (LSOA) closest to the application site (York 017A which covers the Salisbury Terrace area and York 017B which cover the York Central area).</p> <p>This shows that York 017B has:</p> <ul style="list-style-type: none"> <li>• a higher percentage of residents identified as Black/ African/ Caribbean/ Black British in the 2011 Census (2.4% of usual residents in the York Central area compared to 0.6% of usual York residents - LC2109EWIs dataset)</li> </ul>

	<ul style="list-style-type: none"> <li>• a higher percentage of residents identifying as Muslim in the 2011 Census (2.6% of usual residents in the York Central area compared to 1% of usual York residents - LC2107EW dataset)</li> </ul> <p>No significant differences between these two LSOAs and York as a whole were identified when reviewing data on age, sex, long term health problems and disability or provision of unpaid care.</p>
<p>Media reports, articles and research on the private security industry and discriminatory behaviours</p>	<p>Key documents/links reviewed:</p> <p><a href="http://www.risk-uk.com/sia-serves-reminder-to-private-security-industry-over-responsibilities-under-equality-act-2010/">www.risk-uk.com/sia-serves-reminder-to-private-security-industry-over-responsibilities-under-equality-act-2010/</a></p> <p><a href="http://www.voice-online.co.uk/news/features-news/2022/03/31/shopping-while-black/">www.voice-online.co.uk/news/features-news/2022/03/31/shopping-while-black/</a></p> <p><a href="http://www.enar-eu.org/about/islamophobia/">Islamophobia Defined – all Parliamentary Group on British Muslims</a></p> <p><a href="http://www.enar-eu.org/about/islamophobia/">www.enar-eu.org/about/islamophobia/</a></p> <p><a href="https://fra.europa.eu/sites/default/files/fra_uploads/1133-Guide-ethnic-profiling_EN.pdf">https://fra.europa.eu/sites/default/files/fra_uploads/1133-Guide-ethnic-profiling_EN.pdf</a></p> <p><a href="https://journals.sagepub.com/doi/10.1177/1043986219890205">https://journals.sagepub.com/doi/10.1177/1043986219890205</a></p> <p><a href="http://www.asisonline.org/security-management-magazine/articles/2017/04/surveillance-and-stereotypes/">www.asisonline.org/security-management-magazine/articles/2017/04/surveillance-and-stereotypes/</a></p>

### Step 3 – Gaps in data and knowledge

3.1	<b>What are the main gaps in information and understanding of the impact of your proposal? Please indicate how any gaps will be dealt with.</b>	
<b>Gaps in data or knowledge</b>	<b>Action to deal with this</b>	
No data or knowledge gaps were identified	If additional data or information is identified at a later stage, this can be included in a revised Impact Assessment.	

## Step 4 – Analysing the impacts or effects.

4.1	<p><b>Please consider what the evidence tells you about the likely impact (positive or negative) on people sharing a protected characteristic, i.e. how significant could the impacts be if we did not make any adjustments?</b> Remember the duty is also positive – so please identify where the proposal offers opportunities to promote equality and/or foster good relations.</p>		
Equality Groups and Human Rights.	Key findings/impacts	Positive (+) Negative (-) Neutral (0)	High (H) Medium (M) Low (L)
Age	<p>The proposed pedestrian route through the NRM (21/02793/REMM) will enable people who want to travel through the Museum to do so via a direct route from Cinder Lane to Leeman Road to the north-west of the Museum. The route through the Museum has been designed to remain close to the current alignment of Leeman Road. The additional journey length for pedestrians utilising the route through the Museum is 34 metres if using the stepped access and 97 metre if using the ramp.</p> <p>The route through the museum has been designed to offer level surfaces or moderate gradients in line with guidance. The route is 2.4m wide enabling people to pass and meet each other, including people using wheelchairs or mobility aids (including Class 3) with localised reductions for gates and street furniture. This improves on BS8300 guidance of a minimum of 1800mm clear width and a preferred access</p>	Positive and negative	Medium



	<p>route of 2000mm width to accommodate users of larger mobility scooters. A minimum 25mm upstand is to be provided along the pavement length to enable those with mobility canes or similar to identify the pavement edge. The route will be lit and open to pedestrians (including wheelchair users but excluding cyclists who have dismounted and are pushing a bike) during the NRM's opening hours.</p> <p>A direct route is to be provided from the walkway to a set of stairs –level landings and tactile flooring will be present at the top and bottom of the stairs, and handrails will be provided throughout at a height of 900-1000mm from floor level in accordance with BS8300. Steps are to be uniform to provide a consistent and predictable means of access. There are to be two sets of seven steps with a landing in the middle to provide a rest point and sightline break, if necessary.</p> <p>The proposed ramp is longer in design in order to achieve a maximum 1:20 gradient for ease of independent use by wheelchair users and those with mobility equipment, in particular. Level landings are to be provided at the top and bottom of the ramp and at every rise of 500mm, in alignment with BS8300. Two benches, with backrests and with and without armrest provision, are to be provided on ramp landings (approximately 10 metres apart from one another) to provide dwell space. A minimum 1200mm transfer space will also be provided for wheelchair users wishing to transfer onto the bench.</p>		
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	<p>In terms of materiality and colour contrast, a consideration of legibility, identification and ease of use has been provided between trafficked areas and pedestrianised areas, which will be of particular benefit to visitors and staff who are registered blind or partially sighted. High contrast elements will also be provided for tactile flooring and on stair nosings.</p> <p>There may be delays for people wanting to walk through the NRM at peak times, as they have to share the route with visitors to the Museum and comply with security measures in place for the NRM (for example, they will need to submit to bag searches if these are in place). This could have a negative impact for older people and young children who may not be able to stand and wait in a queue for a prolonged period of time.</p> <p>The route may be subject to security checks. Security operatives are required to meet national standards of behaviour for security operatives. This includes the obligation to <i>“act fairly and not discriminate on the grounds of gender, sexual orientation, marital status, race, nationality, ethnicity, religion or beliefs, disability, or any other difference in individuals which is not relevant to the security operatives’ responsibility”</i>. Some young people, especially from minority ethnic groups or people identifying as Muslim (or perceived as such), may however be discouraged from using a route with checkpoints if they fear that they might be subject to discriminatory behaviour as such issues have been reported in the national media.</p>		
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	<p>However, without the route through the Museum, pedestrians would have to use the longer route around the Museum, along Hudson Boulevard and Foundry Way, at all times. As noted in the Inspector's report for the Leeman Road stopping up order, the pedestrian route through Central Hall provides limited mitigation for the loss of Leeman Road as a direct pedestrian route to and from the City Centre and Railway Station.</p> <p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impacts of these decisions are therefore not assessed here.</p>		
<p><b>Disability</b></p>	<p>The proposed pedestrian route through the NRM will enable people who want to travel through the Museum to do so via a direct route from Cinder Lane to Leeman Road to the north-west of the Museum. The route through the Museum has been designed to remain close to the current alignment of Leeman Road. The additional journey length for pedestrians utilising the route through the Museum is 34 metres if using the stepped access and 97 metre if using the ramp.</p> <p>The route through the museum has been designed to offer level surfaces or moderate gradients in line with guidance. The route is 2.4m wide enabling people to pass and meet each other, including people using wheelchairs or mobility</p>	<p><b>Positive and negative</b></p>	<p><b>Medium</b></p>

	<p>aids (including Class 3) with localised reductions for gates and street furniture. This improves on BS8300 guidance of a minimum of 1800mm clear width and a preferred access route of 2000mm width to accommodate users of larger mobility scooters. A minimum 25mm upstand is to be provided along the pavement length to enable those with mobility canes or similar to identify the pavement edge. The route will be lit and open to pedestrians (including wheelchair users but excluding cyclists who have dismounted and are pushing a bike) during the NRM's opening hours.</p> <p>A direct route is to be provided from the walkway to a set of stairs –Level landings and tactile flooring will be present at the top and bottom of the stairs, and handrails will be provided throughout at a height of 900-1000mm from floor level in accordance with BS8300. Steps are to be uniform to provide a consistent and predictable means of access. There are to be two sets of seven steps with a landing in the middle to provide a rest point and sightline break, if necessary.</p> <p>The proposed ramp is longer in design in order to achieve a maximum 1:20 gradient for ease of independent use by wheelchair users and those with mobility equipment, in particular. Level landings are to be provided at the top and bottom of the ramp and at every rise of 500mm, in alignment with BS8300. Two benches, with backrests and with and without armrest provision, are to be provided on ramp landings (approximately 10 metres apart from one another) to provide dwell space. A minimum 1200mm transfer space</p>		
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	<p>will also be provided for wheelchair users wishing to transfer onto the bench.</p> <p>In terms of materiality and colour contrast, a consideration of legibility, identification and ease of use has been provided between trafficked areas and pedestrianised areas, which will be of particular benefit to visitors and staff who are registered blind or partially sighted. High contrast elements will also be provided for tactile flooring and on stair nosings.</p> <p>The route offers level access into the Museum on the Cinder Lane side and includes stairs and a ramp on the other side of the Museum. By comparison, Leeman Road currently has high volumes of traffic, a narrow 1.5m footway, lacks resting places, natural overlooking and has a gradient of 1:16 in some sections</p> <p>To the north of the Museum, the route is designed to be identified clearly, using the stairs or ramp option, and then following a kerbed footway (opposite the disabled car parking spaces) to the site boundary to the north-west.</p> <p>The approach from Cinder Lane is not yet fully designed as the detail design for this area is not part of this planning application. As the route crosses an open plaza area, the needs of disabled users will need to be considered when this design is developed, particularly the need of users who are blind or visually impaired.</p>		
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	<p>The route inside Central Hall does not provide any tactile guidance for users who are blind or visually impaired. The route through Central Hall may also be difficult to navigate, especially at busy times, for people who suffer from mental health conditions, neurodevelopmental disorders, or learning disabilities. The NRM has indicated that suitable signage would be provided and that staff would be available to direct users through the building.</p> <p>There may be delays for people wanting to walk through the NRM at peak times, as they have to share the route with visitors to the Museum and comply with security measures in place for the NRM (for example, they will need to submit to bag searches if these are in place). This could have a negative impact for some people with disabilities, who may not be able to stand and wait in a queue for a prolonged period of time. Some people may also be discouraged from using the route due to the journey time uncertainty associated with possible queues and security measures.</p> <p>However, without the route through the Museum, pedestrians would have to use the longer route around the Museum, along Hudson Boulevard and Foundry Way, at all times. As noted in the Inspector's report for the Leeman Road stopping up order, the pedestrian route through Central Hall provides limited mitigation for the loss of Leeman Road as a direct pedestrian route to and from the City Centre and Railway Station.</p>		
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	<p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impacts of these decisions are therefore not assessed here.</p> <p>Cyclists (even dismounted and pushing their bike) and people who use their cycle as a mobility aid will not be permitted to use the route through Central Hall. This is however not considered here as the principle of the route through the Museum being available only to pedestrians was set under 18/01884/OUTM.</p>		
<p><b>Gender</b></p>	<p>The proposed pedestrian route through the NRM will enable people who want to travel through the Museum to do so via a direct route from Cinder Lane to Leeman Road to the north-west of the Museum.</p> <p>The walkway route will be well lit and will benefit from natural surveillance during the Museum's opening hours, when visitors to the Museum will also be coming and going. There will also be a security presence at the building and CCTV surveillance. The proposals were found to be acceptable by North Yorkshire Police Designing Out Crime Officer.</p> <p>No positive or negative impacts have been identified in relation to gender characteristics.</p>	<p><b>Neutral</b></p>	<p><b>n/a</b></p>

	<p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impact of these decisions are therefore not assessed here.</p>		
<p><b>Gender Reassignment</b></p>	<p>The proposed pedestrian route through the NRM will enable people who want to travel through the Museum to do so via a direct route from Cinder Lane to Leeman Road to the north-west of the Museum.</p> <p>The walkway route will be well lit and will benefit from natural surveillance during the Museum's opening hours, when visitors to the Museum will also be coming and going. There will also be a security presence at the building and CCTV surveillance. The proposals were found to be acceptable by North Yorkshire Police Designing Out Crime Officer.</p> <p>The route may be subject to security checks. Security operatives are required to meet national standards of behaviour for security operatives. This includes the obligation to <i>“act fairly and not discriminate on the grounds of gender, sexual orientation, marital status, race, nationality, ethnicity, religion or beliefs, disability, or any other difference in individuals which is not relevant to the security operatives’ responsibility”</i>. Some users who have received gender reassignment surgery or are in a gender reassignment process may however be discouraged from using a route</p>	<p><b>Positive and negative</b></p>	<p><b>Medium</b></p>



	<p>with checkpoints if they fear that they might be subject to discriminatory behaviour as such issues have been reported in the national media.</p> <p>However, without the route through the Museum, pedestrians would have to use the longer route around the Museum, along Hudson Boulevard and Foundry Way, at all times. As noted in the Inspector's report for the Leeman Road stopping up order, the pedestrian route through Central Hall provides limited mitigation for the loss of Leeman Road as a direct pedestrian route to and from the City Centre and Railway Station.</p> <p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impact of these decisions are therefore not assessed here.</p>		
<p><b>Marriage and civil partnership</b></p>	<p>The proposed pedestrian route through the NRM will enable people who want to travel through the Museum to do so via a direct route from Cinder Lane to Leeman Road to the north-west of the Museum. The route will be lit and open to pedestrians (including wheelchair users but excluding cyclists who have dismounted and are pushing a bike) during the NRM's opening hours.</p> <p>No positive or negative impacts have been identified in relation to marriage and civil partnership characteristics.</p>	<p><b>Neutral</b></p>	<p><b>n/a</b></p>

	<p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impacts of these decisions are therefore not assessed here.</p>		
<p><b>Pregnancy and maternity</b></p>	<p>The proposed pedestrian route through the NRM (21/02793/REMM) will enable people who want to travel through the Museum to do so via a direct route from Cinder Lane to Leeman Road to the north-west of the Museum.</p> <p>The route through the Museum has been designed to offer level surfaces or moderate gradients in line with guidance. The route is 2.4m wide with localised reductions for gates and street furniture enabling people to pass and meet each other, including people using wheelchairs, mobility aids, and pushchairs. The route will be lit and open to pedestrians (including wheelchair users but excluding cyclists who have dismounted and are pushing a bike) during the NRM's opening hours.</p> <p>By comparison, Leeman Road currently has high volumes of traffic, a narrow 1.5m footway, lacks resting places, natural overlooking and has a gradient of 1:16 in some sections</p> <p>There may be delays for people wanting to walk through the NRM at peak times, as they have to share the route with visitors to the Museum and comply with security measures in place for the NRM (for example, they will need to submit to</p>	<p><b>Positive and negative</b></p>	<p><b>Medium</b></p>

	<p>bag searches if these are in place). This could have a negative impact for users with reduced mobility due to pregnancy and for those with babies and very young children as they may not be able to stand and wait in a queue for a prolonged period of time.</p> <p>However, without the route through the Museum, pedestrians would have to use the longer route around the Museum, along Hudson Boulevard and Foundry Way, at all times. As noted in the Inspector's report for the Leeman Road stopping up order, the pedestrian route through Central Hall provides limited mitigation for the loss of Leeman Road as a direct pedestrian route to and from the City Centre and Railway Station.</p> <p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impacts of these decisions are therefore not assessed here.</p>		
<b>Race</b>	The review of the Census 2011 data for the two Lower Super Output Areas (LSOA) closest to the application site (York 017A which covers the Salisbury Terrace area and York 017B which cover the York Central area) shows that York 017B has a higher percentage of residents identified as	<b>Positive and negative</b>	<b>Medium</b>

	<p>Black/ African/ Caribbean/ Black British in the 2011 Census (2.4% of usual residents in the York Central area compared to 0.6% of usual York residents - LC2109EWIs dataset).</p> <p>The proposed pedestrian route through the NRM will enable people who want to travel through the Museum to do so via a direct route from Cinder Lane to Leeman Road to the north-west of the Museum.</p> <p>The walkway route will be well lit and will benefit from natural surveillance during the Museum's opening hours, when visitors to the Museum will also be coming and going. There will also be a security presence at the building and CCTV surveillance. The proposals were found to be acceptable by North Yorkshire Police Designing Out Crime Officer.</p> <p>The route may be subject to security checks. Security operatives are required to meet national standards of behaviour for security operatives. This includes the obligation to <i>“act fairly and not discriminate on the grounds of gender, sexual orientation, marital status, race, nationality, ethnicity, religion or beliefs, disability, or any other difference in individuals which is not relevant to the security operatives’ responsibility”</i>. Some minority ethnic users may however be discouraged from using a route with checkpoints the route through Central Hall if they fear that they might be subject to discriminatory behaviour as such issues have been reported in the national media.</p> <p>However, without the route through the Museum, pedestrians would have to use the longer route around the Museum,</p>		
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	<p>along Hudson Boulevard and Foundry Way, at all times. As noted in the Inspector's report for the Leeman Road stopping up order, the pedestrian route through Central Hall provides limited mitigation for the loss of Leeman Road as a direct pedestrian route to and from the City Centre and Railway Station.</p> <p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impacts of these decisions are therefore not assessed here.</p>		
<p><b>Religion and belief</b></p>	<p>The review of the Census 2011 data for the two Lower Super Output Areas (LSOA) closest to the application site (York 017A which covers the Salisbury Terrace area and York 017B which cover the York Central area) shows that York 017B has a higher percentage of residents identifying as Muslim in the 2011 Census (2.6% of usual residents in the York Central area compared to 1% of usual York residents - LC2107EW dataset).</p> <p>The proposed pedestrian route through the NRM will enable people who want to travel through the Museum to do so via a</p>	<p><b>Positive and negative</b></p>	<p><b>Medium</b></p>

	<p>direct route from Cinder Lane to Leeman Road to the north-west of the Museum.</p> <p>The walkway route will be well lit and will benefit from natural surveillance during the Museum's opening hours, when visitors to the Museum will also be coming and going. There will also be a security presence at the building and CCTV surveillance. The proposals were found to be acceptable by North Yorkshire Police Designing Out Crime Officer.</p> <p>The route may be subject to security checks. Security operatives are required to meet national standards of behaviour for security operatives. This includes the obligation to <i>“act fairly and not discriminate on the grounds of gender, sexual orientation, marital status, race, nationality, ethnicity, religion or beliefs, disability, or any other difference in individuals which is not relevant to the security operatives' responsibility”</i>. Some users from the Muslim community may however be discouraged from using the route through Central Hall a route with checkpoints if they fear that they might be subject to discriminatory behaviour as such issues have been reported in the national media.</p> <p>However, without the route through the Museum, pedestrians would have to use the longer route around the Museum, along Hudson Boulevard and Foundry Way, at all times. As noted in the Inspector's report for the Leeman Road stopping up order, the pedestrian route through Central Hall provides limited mitigation for the loss of Leeman Road as a direct</p>		
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	<p>pedestrian route to and from the City Centre and Railway Station.</p> <p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impacts of these decisions are therefore not assessed here.</p>		
<p><b>Sexual orientation</b></p>	<p>The proposed pedestrian route through the NRM will enable people who want to travel through the Museum to do so via a direct route from Cinder Lane to Leeman Road to the north-west of the Museum.</p> <p>The walkway route will be well lit and will benefit from natural surveillance during the Museum's opening hours, when visitors to the Museum will also be coming and going. There will also be a security presence at the building and CCTV surveillance. The proposals were found to be acceptable by North Yorkshire Police Designing Out Crime Officer.</p> <p>The route may be subject to security checks. Security operatives are required to meet national standards of behaviour for security operatives. This includes the obligation to <i>“act fairly and not discriminate on the grounds of gender, sexual orientation, marital status, race, nationality, ethnicity, religion or beliefs, disability, or any other difference in individuals which is not relevant to the security operatives”</i></p>	<p><b>Positive and negative</b></p>	<p><b>Medium</b></p>

	<p><i>responsibility</i>". Some LGBTQ+ users may however be discouraged from using a route with checkpoints if they fear that they might be subject to discriminatory behaviour as such issues have been reported in the national media.</p> <p>However, without the route through the Museum, pedestrians would have to use the longer route around the Museum, along Hudson Boulevard and Foundry Way, at all times. As noted in the Inspector's report for the Leeman Road stopping up order, the pedestrian route through Central Hall provides limited mitigation for the loss of Leeman Road as a direct pedestrian route to and from the City Centre and Railway Station.</p> <p>The closure of Leeman Road was approved under 18/01884/OUTM and through the stopping up process and is therefore not considered in this assessment. The same applies to the alignment of the new roads, and the pedestrian and cycle route - their design is covered by 20/00710/REMM. The impacts of these decisions are therefore not assessed here.</p>		
<p><b>Other Socio-economic groups including :</b></p>	<p><b>Could other socio-economic groups be affected e.g. carers, ex-offenders, low incomes?</b></p>		



<b>Carer</b>	Impacts on carers are similar to those noted above for people living with disabilities.		
<b>Low income groups</b>	No identified impacts.		
<b>Veterans, Armed Forces Community</b>	No identified impacts.		
<b>Impact on human rights:</b>			
List any human rights impacted.	No human rights impacts identified.		

**Use the following guidance to inform your responses:**

Indicate:

- Where you think that the proposal could have a **POSITIVE** impact on any of the equality groups like promoting equality and equal opportunities or improving relations within equality groups
- Where you think that the proposal could have a **NEGATIVE** impact on any of the equality groups, i.e. it could disadvantage them
- Where you think that this proposal has a **NEUTRAL** effect on any of the equality groups listed below i.e. it has no effect currently on equality groups.

It is important to remember that a proposal may be highly relevant to one aspect of equality and not relevant to another.

EIA 02/2021



<p><b>High impact</b> (The proposal or process is very equality relevant)</p>	<p>There is significant potential for or evidence of adverse impact The proposal is institution wide or public facing The proposal has consequences for or affects significant numbers of people The proposal has the potential to make a significant contribution to promoting equality and the exercise of human rights.</p>
<p><b>Medium impact</b> (The proposal or process is somewhat equality relevant)</p>	<p>There is some evidence to suggest potential for or evidence of adverse impact The proposal is institution wide or across services, but mainly internal The proposal has consequences for or affects some people The proposal has the potential to make a contribution to promoting equality and the exercise of human rights</p>
<p><b>Low impact</b> (The proposal or process might be equality relevant)</p>	<p>There is little evidence to suggest that the proposal could result in adverse impact The proposal operates in a limited way The proposal has consequences for or affects few people The proposal may have the potential to contribute to promoting equality and the exercise of human rights</p>

## Step 5 - Mitigating adverse impacts and maximising positive impacts

5.1	<b>Based on your findings, explain ways you plan to mitigate any unlawful prohibited conduct or unwanted adverse impact. Where positive impacts have been identified, what is been done to optimise opportunities to advance equality or foster good relations?</b>
<p>In addition to the design features of the proposals for Central Hall and the associated walkway route identified above, the following measures are proposed:</p> <p>Queuing - Upon opening the museum, a dedicated member of staff is assigned to meet and greet visitors. This will include local residents seeking a passage through Central Hall. Their role is to implement queue-busting techniques, which will include identifying residents and enabling them to pass quickly through the building, along with catering for any specific user needs e.g. toilet visits, seating for the elderly or pregnant women as they do now.</p> <p>Wayfinding and navigation - Visitors planning a trip to Central Hall will be able to familiarise themselves with the layout of Central Hall and adjoining areas on the museum's website prior to arrival. The aim of this is to encourage confidence and autonomy for those with sensory and cognitive impairments and access requirements, in particular. Multimedia videos will be described via audio and also captioned to support D/deaf and hard of hearing users.</p> <p>Signage within Central Hall will follow the 'two-sense' approach, providing both visual and tactile access to visitors and staff. The wayfinding scheme will ensure legibility (with strong contrast and of appropriate scale and clarity of content). Where appropriate, key services will also be indicated with proximity markers. Mapping will be supported with access to further information in other formats through QR code or similar online.</p>	

Contrast elements between walls, floors and furniture have been intrinsically considered within the internal design of Central Hall, promoting an ease of identification and navigation for partially sighted and neurodivergent individuals, in particular.

The frequent availability, inclusive design and materiality of seating - at appropriate heights and with back and armrests is also being incorporated to provide a comfortable experience for those who may require frequent dwell points.

Staff training - The operational elements of the experience will also be vital to ensure inclusivity to D/deaf and disabled people and those with other protected characteristics. An ongoing disability and equality training programme is currently utilised for all front-facing staff members, and includes impairment and access requirement specific modules such as dementia friendly training etc. Staff will always be on hand within Central Hall to support D/deaf and disabled people, if required, whether they are visiting the museum or passing through

Route availability - The opening hours of NRM will revert to seven days a week by May 2023

Planning Conditions - recommended conditions of the reserved matters application will ensure that mitigation is delivered:

- Condition 6 (Hard landscaping and street furniture)
- Condition 8 (signage strategy)
- Condition 11 (lighting strategy)

## Step 6 – Recommendations and conclusions of the assessment

6.1	<p><b>Having considered the potential or actual impacts you should be in a position to make an informed judgement on what should be done. In all cases, document your reasoning that justifies your decision. There are four main options you can take:</b></p>
<ul style="list-style-type: none"> <li>- <b>No major change to the proposal</b> – the EIA demonstrates the proposal is robust. There is no potential for unlawful discrimination or adverse impact and you have taken all opportunities to advance equality and foster good relations, subject to continuing monitor and review.</li> <li>- <b>Adjust the proposal</b> – the EIA identifies potential problems or missed opportunities. This involves taking steps to remove any barriers, to better advance quality or to foster good relations.</li> <li>- <b>Continue with the proposal</b> (despite the potential for adverse impact) – you should clearly set out the justifications for doing this and how you believe the decision is compatible with our obligations under the duty</li> <li>- <b>Stop and remove the proposal</b> – if there are adverse effects that are not justified and cannot be mitigated, you should consider stopping the proposal altogether. If a proposal leads to unlawful discrimination it should be removed or changed.</li> </ul> <p><b>Important:</b> If there are any adverse impacts you cannot mitigate, please provide a compelling reason in the justification column.</p>	
<b>Option selected</b>	<b>Conclusions/justification</b>
Continue with the proposal	This Equality Impact Assessment report presents the equality issues potentially arising from the planning application 21/02793/REMM, focusing on the proposed route through the Museum.

It reports committed and reasonable proposed mitigation measures to mitigate negative effects and positive actions to enhance equality outcomes. These are described above and include:

- The route through the museum has been designed to offer level surfaces or moderate gradients in line with guidance;
- The route is 2.4m wide enabling people to pass and meet each other, including people using wheelchairs or mobility aids (including Class 3) with localised reductions for gates and street furniture. This improves on BS8300 guidance of a minimum of 1800mm clear width and a preferred access route of 2000mm width to accommodate users of larger mobility scooters;
- A minimum 25mm upstand is to be provided along the pavement length to enable those with mobility canes or similar to identify the pavement edge;
- A direct route is to be provided from the walkway to a set of stairs –level landings and tactile flooring will be present at the top and bottom of the stairs, and handrails will be provided throughout at a height of 900-1000mm from floor level in accordance with BS8300;
- Steps are to be uniform to provide a consistent and predictable means of access. There are to be two sets of seven steps with a landing in the middle to provide a rest point and sightline break, if necessary;
- The proposed ramp is longer in design in order to achieve a maximum 1:20 gradient for ease of independent use by wheelchair users and those with mobility equipment, in particular. Level landings are to be provided at the top and bottom of the ramp and at every rise of 500mm, in alignment with BS8300. Two benches, with backrests with and without armrest provision, are to be provided on ramp landings

	<p>(approximately 10 metres apart from one another) to provide dwell space. A minimum 1200mm transfer space will also be provided for wheelchair users wishing to transfer onto the bench;</p> <ul style="list-style-type: none"><li>• The route will be lit and open to pedestrians (including wheelchair users but excluding cyclists who have dismounted and are pushing a bike) during the NRM's opening hours;</li><li>• The additional journey length for pedestrians utilising the route through the Museum is 34 metres if using the stepped access and 97 metre if using the ramp. The route offers level access into the Museum on the Cinder Lane side and includes stairs and a ramp on the other side of the Museum. By comparison, Leeman Road currently has high volumes of traffic, a narrow 1.5m footway, lacks resting places, natural overlooking and has a gradient of 1:16 in some sections;</li><li>• Dedicated staff assigned to meet and greet visitors, including local residents;</li><li>• Wayfinding and navigation measures both online and within Central Hall;</li><li>• Frequent availability of seating along the route;</li><li>• Disability and equality training programme for all front-facing staff members.</li></ul>
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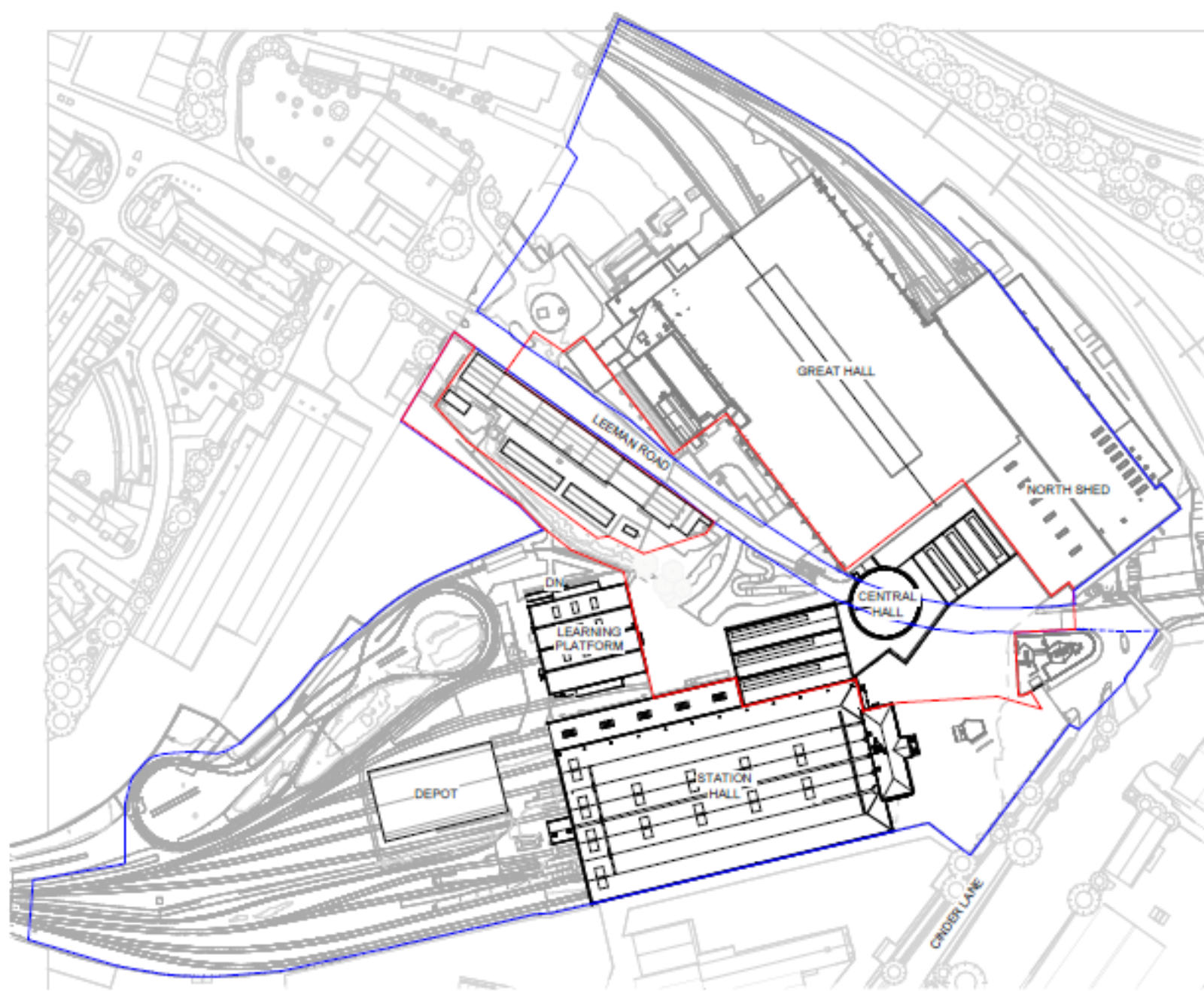
# Planning Committee A

To be held on Thursday 4<sup>th</sup> August 2022 at 4:30pm

# 21/02793/REMM – Railway Museum Annexe, Leeman Road, York

Reserved matters application for layout, scale, appearance, landscaping and access for the construction of Central Hall (F1 use class) including entrance hall, exhibition space and café with associated access, parking, landscaping and external works following the demolition of the mess room and other structures pursuant to 18/01884/OUTM

# Site Location Plan





View looking East down Leeman Road towards Marble Arch (Bullnose building on Right)







Approximate Area where Central Gallery would be linked to Great Hall



View looking East down Leeman Road with Station Hall on right and Great Hall on left





Area Proposed for Futures Gallery and Ramped Access to North



View looking East down Leeman Road



View looking West up Leeman Road where disabled parking and service access would be located



Existing site entrance to be demolished



## Former mess room to be demolished



## Bullnose Building to be retained

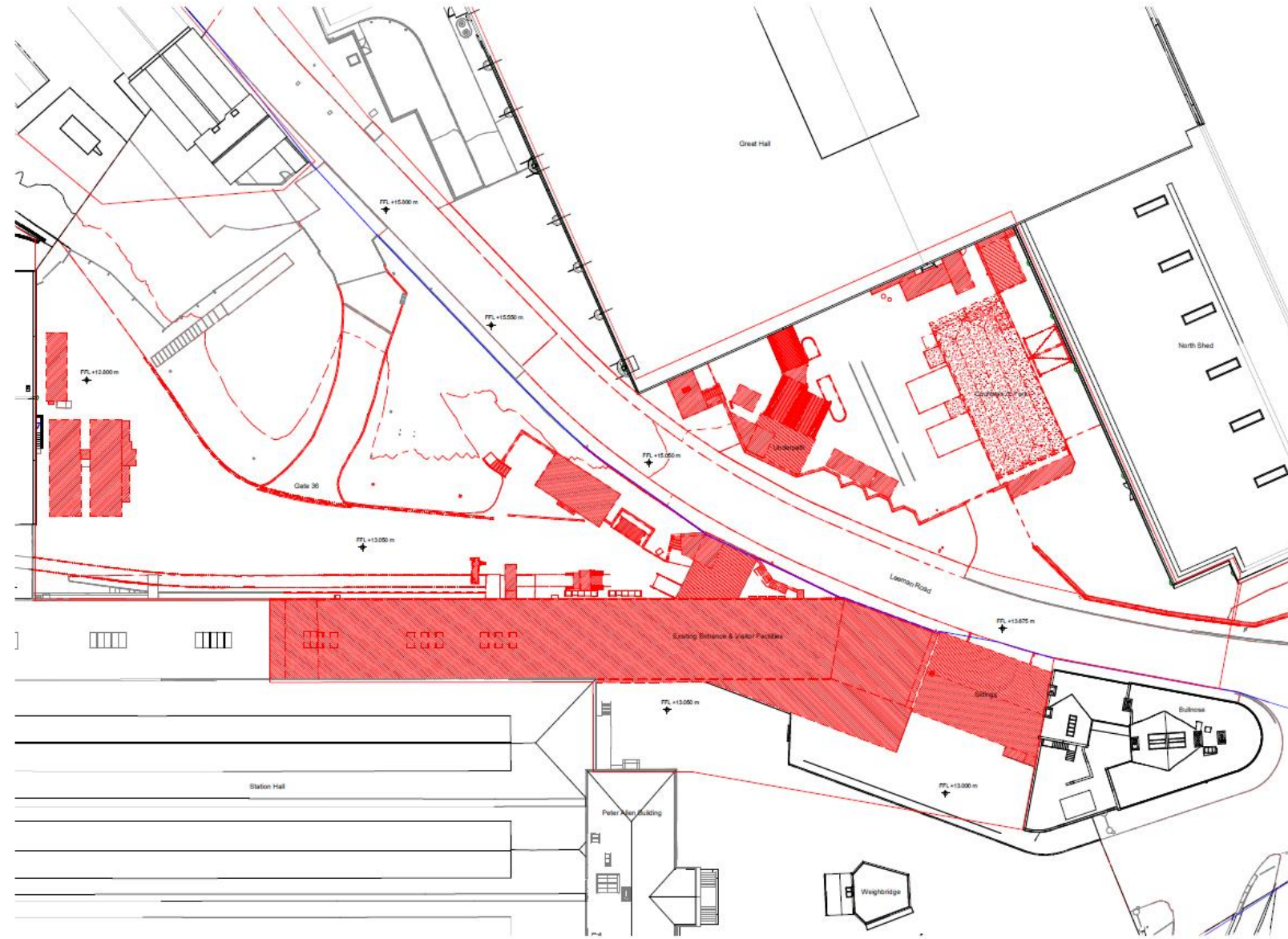




Rear of modern buildings to be demolished

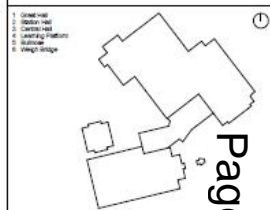


# Proposed Demolition Plan



General Note:  
 1. All dimensions are in millimetres unless stated otherwise.  
 2. All dimensions are to be verified on site before commencing with the work.  
 3. Further dimensions shall be verified on site if any discrepancies arise.  
 4. There are a number of Contractor Safety Measures (CSMs) within the design drawings and specifications. Refer to list of items within specification.  
 5. Existing structural partitions are indicated only. They were not included in the structural survey.  
 6. All mechanical and electrical services shown on this drawing is purely indicative. Please refer to the design engineer's drawings.

- Landscape Remodelling
- - - Proposed Demolitions
- ▣ Proposed Relocations



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Rev: 0001 Issued for Planning

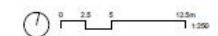
Author:  
**FEILDEN FOWLES**  
 Feilden Fowles Architects  
 2 Kings Square  
 London EC2A 4DF, UK  
 www.feildenfowles.co.uk  
 Tel: +44 (0)20 7522 4544  
 info@feildenfowles.co.uk  
 © Feilden Fowles

Client:  
**National Railway Museum**  
 Site Address:  
 Leeman Rd, York YO01 4JU

Project:  
**118 / CHY - Central Hall**

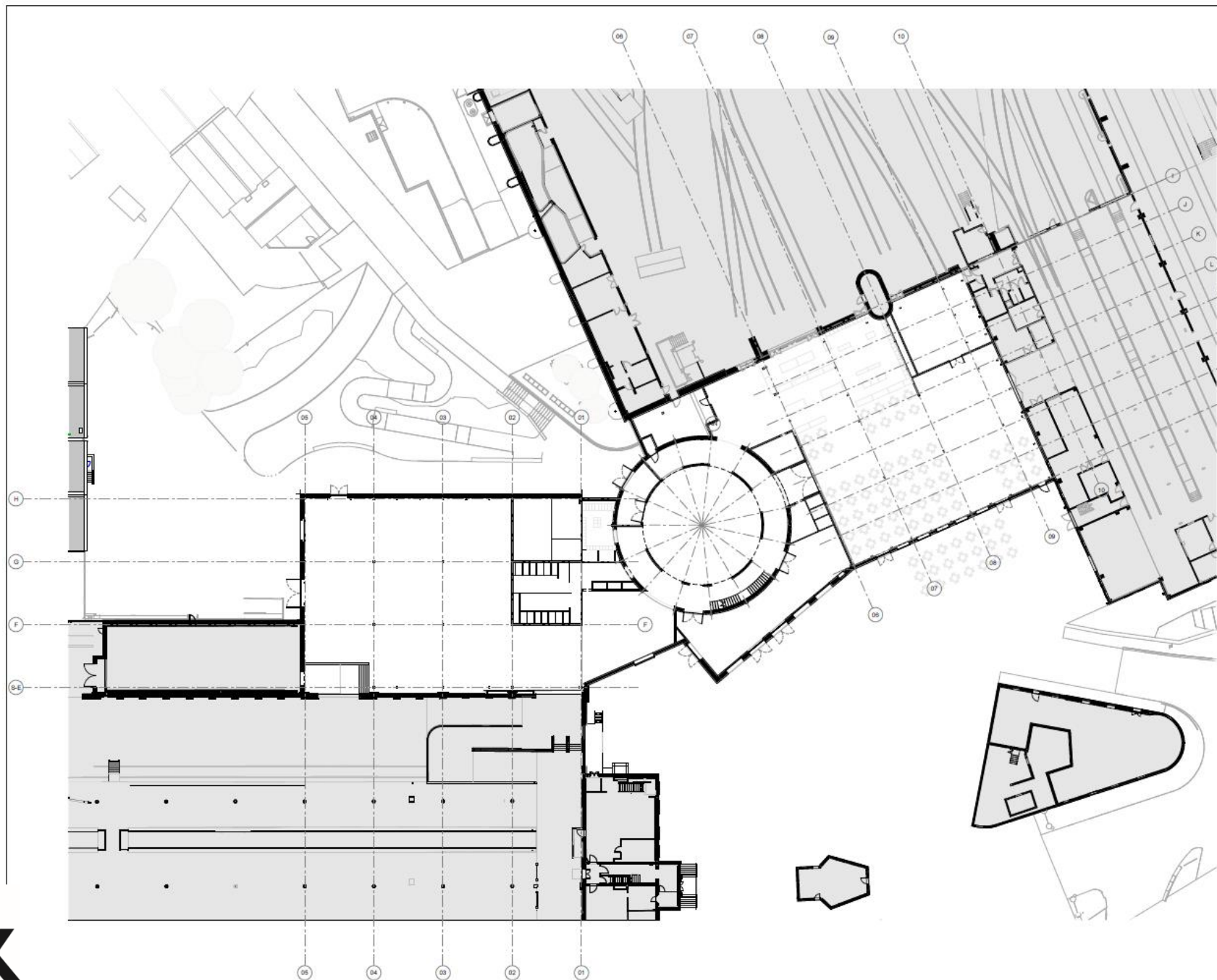
Drawing Title:  
**Demolition Plan**

Project Issue: <b>Stage 3</b>	Date: <b>20/12/21</b>
Scale @ A1: <b>1:500</b>	Scale @ A0: <b>1:250</b>
Drawn: <b>FFA</b>	Checked: <b>FFA</b>
Drawing Number: <b>NRM-FFA-CH-00-DR-A-00500</b>	Revision: <b>P1-1</b>

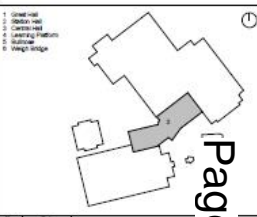




# Proposed Ground Floor Plan



General Notes:  
 1. All dimensions are in millimetres unless stated otherwise.  
 2. All dimensions shall be verified on site before commencing with the work.  
 3. Make any amendments to be noted on any drawings.  
 4. Make any amendments to be noted on any drawings.  
 5. Make any amendments to be noted on any drawings.  
 6. Make any amendments to be noted on any drawings.  
 7. Make any amendments to be noted on any drawings.  
 8. Make any amendments to be noted on any drawings.  
 9. Make any amendments to be noted on any drawings.  
 10. Make any amendments to be noted on any drawings.



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REV	DATE	DESCRIPTION
1	22/10/2021	Issued for Planning
2	22/10/2021	Re-issued for Planning
3	22/10/2021	Updated boundary line

Architect:  
**FEILDEN FOWLES**  
 116 / CHY - Central Hall  
 116 / CHY - Central Hall  
 116 / CHY - Central Hall

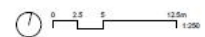
Client:  
**National Railway Museum**

Site Address:  
 Leeman Rd, York YO26 4XJ

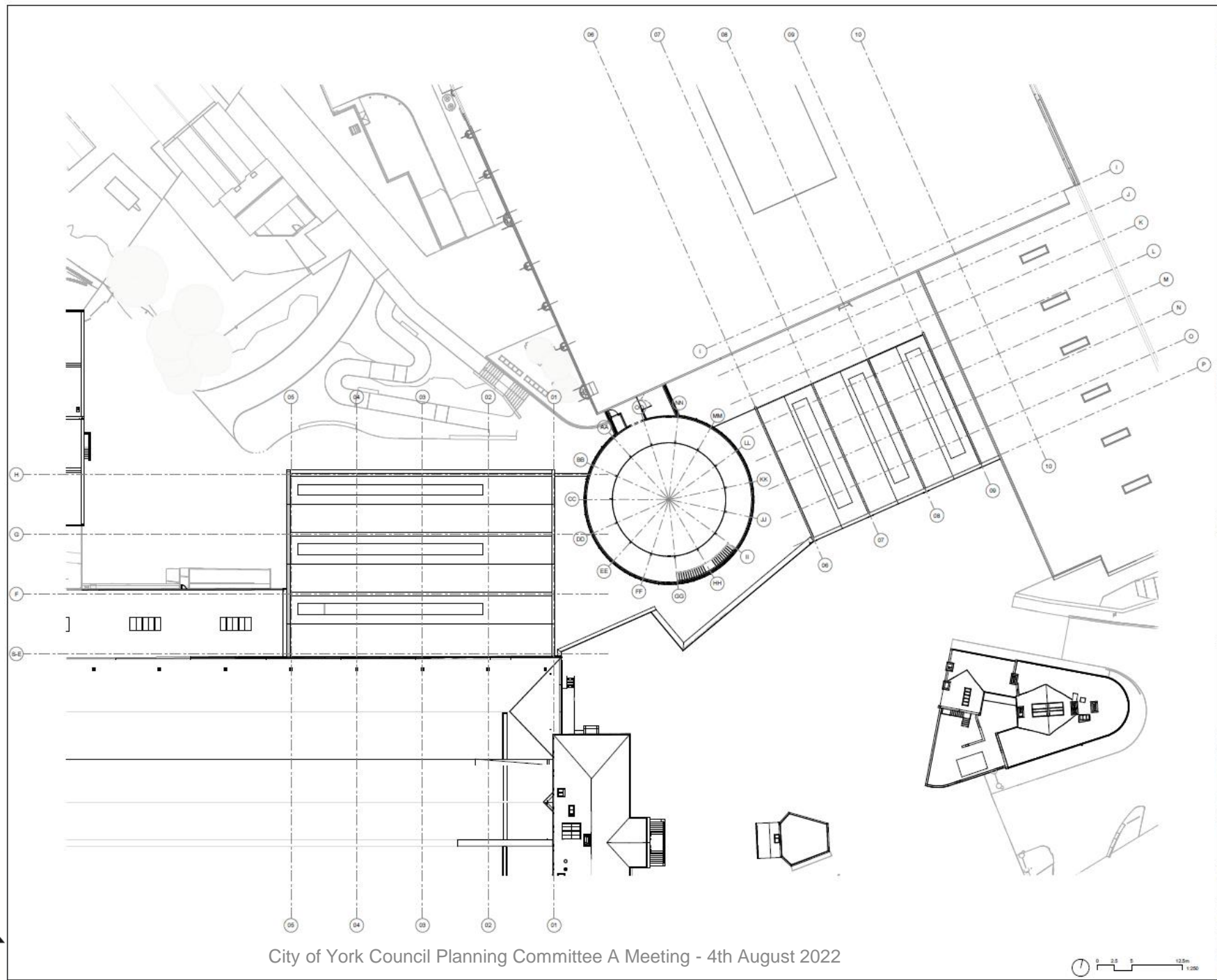
Project:  
 116 / CHY - Central Hall

Drawing Title:  
**Proposed Ground Floor Plan**

Project Ref: Stage 3  
 Date: 31/05/22  
 Scale: 1:500  
 Date: 19/05/22  
 Scale: 1:250  
 Date: 31/05/22  
 Scale: 1:250  
 Date: 31/05/22  
 Scale: 1:250



# Proposed First Floor Plan



**General Notes:**

- All dimensions are in millimetres unless stated otherwise.
- Dimensions are to be verified on site before proceeding with the work.
- Levels are shown unless otherwise stated on drawings.
- There are no structural or construction details shown on this drawing and specifications. Refer to all drawings with construction details.
- Existing ground levels are indicated on drawings. They were not included in the proposed building.
- All structural and technical details shown on this drawing are purely indicative. Please refer to the relevant drawings.

**Legend:**

- 1. Existing wall
- 2. Existing wall
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- 100. Existing wall

**Revision Table:**

REV	DATE	DESCRIPTION
1	12/03/2022	Issued for Planning
2	16/05/2022	Issued for Planning
3	12/04/2022	Updated boundary line

**Project Information:**

**Architect:** FEILDEN FOWLES

**Project Name:** National Railway Museum

**Site Address:** Leeman Rd, York YO26 6XJ

**Project:** 116 / CHY - Central Hall

**Drawing Title:** Proposed First Floor Plan

**Project Status:** Stage 3

**Date:** 31/05/22

**Scale:** 1:500

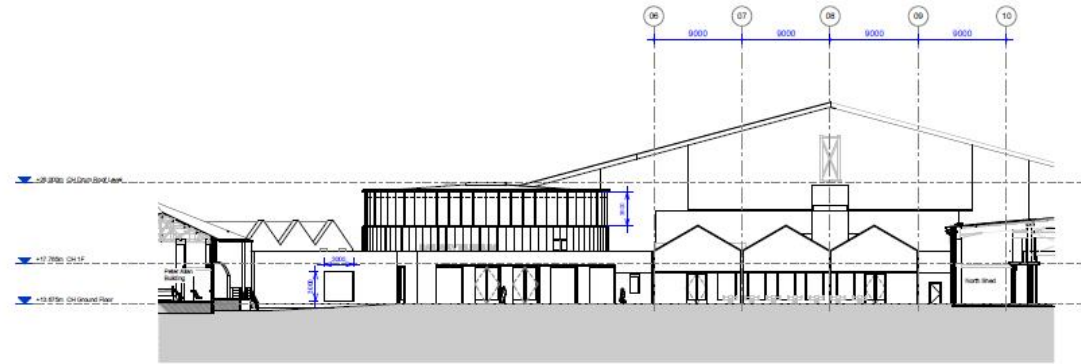
**Check:** FFA

**Drawing Number:** NRM-FFA-CH-01-DR-A-01101

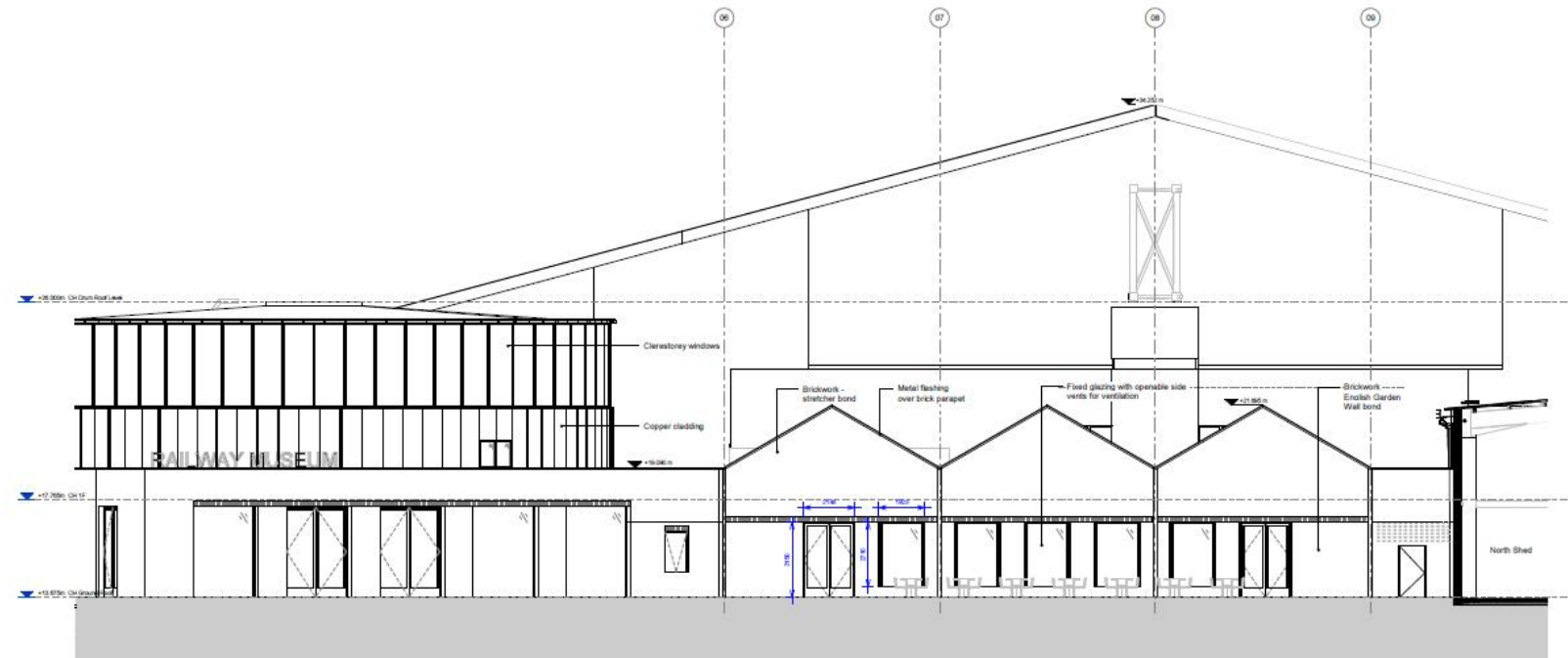
**Revision:** P2-3



# Proposed Elevations – Museum Square



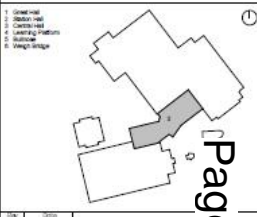
1 Proposed Museum Square Elevation  
1: 250



2 Proposed Museum Square Elevation Callout  
1: 100

City of York Council Planning Committee A Meeting - 4th August 2022

General Notes:  
1. All dimensions are in millimetres unless stated otherwise.  
2. All dimensions are to be verified on site before commencing with the work.  
3. Hidden features shall be notified in writing if any discrepancies.  
4. There are a number of Contaminated Land (CDL) sites within the design drawings and specifications. Refer to list of items within specifications.  
5. Existing ground conditions are shown on site. They have not included in the proposed activity.  
6. The drawings are for information only. They are not to be used for construction. Please refer to the relevant contract documents for the drawing or project information. Please refer to the relevant drawings.



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1 22/10/2021 Issued for Planning

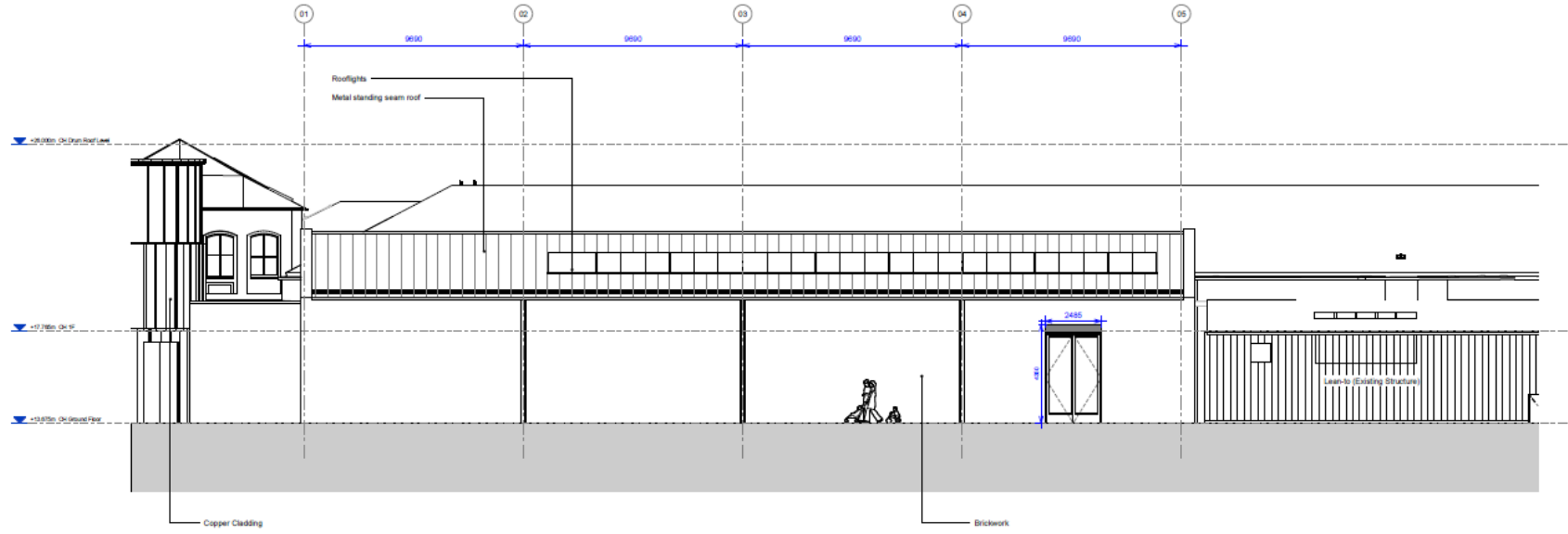
Architect:  
**FEILDEN FOWLES**  
Public Contact Details:  
01904 31000  
London SW11 1TL  
www.feildenfowles.co.uk  
© Feilden Fowles

Client:  
**National Railway Museum**  
Site Address:  
Leeman Rd, York YO26 4XJ  
Project:  
**116 / CHY - Central Hall**

Drawing Title: <b>Proposed Museum Square Elevation</b>		Date: <b>20/12/21</b>
Project Ref: <b>Stage 3</b>	Scale @ A3: <b>N/A 2:1</b>	Drawn: <b>FFA</b>
Scale @ A1: <b>N/A</b>	As indicated	Checked: <b>FFA</b>
Drawing Number: <b>NRM-FFA-CH-XX-DR-A-02101</b>	Revision: <b>P1-1</b>	



# Proposed Elevations – North Approach



General Notes:  
 1. All dimensions are in millimetres unless stated otherwise.  
 2. All dimensions shall be verified on site before commencing with the work.  
 3. Details shown shall be subject to the approval of any authorities.  
 4. There are a number of Contractor Obligations (COs) within the design drawings and specifications. Refer to all COs in areas with identification.  
 5. Existing works are shown as indicated only. They were not included in the measured survey.  
 6. All structural and technical services shown on the drawing to justify inclusive. Please refer to the Engineer's drawings.



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1: 20/12/2021 Issued for Planning

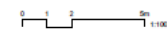
Address:  
**FEILDEN FOWLES**  
 Feilden Fowles Architects  
 2 Stage Street  
 London SE1 1LL  
 www.feildenfowles.co.uk  
 +44 (0)20 7522 4544  
 info@feildenfowles.co.uk  
 020 7522 4544

Client:  
**National Railway Museum**  
 Site Address:  
 Leeman Rd, York YO26 4JU

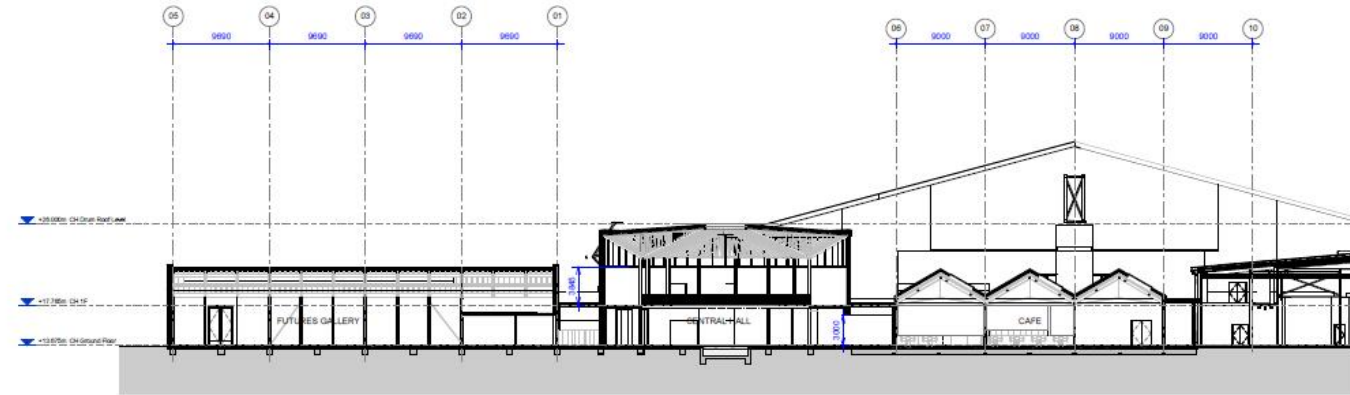
Project:  
**116 / CHY - Central Hall**

Drawing Title:  
**Proposed North Approach Elevation**

Project Status: <b>Stage 3</b>	Date: <b>20/12/21</b>
Scale @ A1: <b>1 : 200</b>	Scale @ A2: <b>1 : 100</b>
Drawing Number: <b>2</b>	Revision: <b>P1-1</b>



# Proposed Long Section – West to East



General Notes:  
 1. All dimensions are in millimetres unless stated otherwise.  
 2. All dimensions are to be taken as indicated on drawings unless otherwise stated.  
 3. Unless stated otherwise, all dimensions are to be taken as indicated on drawings unless otherwise stated.  
 4. There are a number of Contract Documents (CDs) which apply to this design and are available on the project website.  
 5. Drawing is for information only. It is not to be used for construction purposes.  
 6. All dimensions and locations are shown on the drawing in purple ink. Please refer to the Engineering Drawings.

- 1. Steel wall
- 2. Brick wall
- 3. Concrete wall
- 4. Masonry wall
- 5. Timber wall
- 6. Glass wall



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REV	NO	DESCRIPTION
1	01/10/2021	Issued for Planning

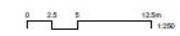
Architect:  
**FEILDEN FOWLES**  
 Feilden, Cowie, Architects  
 2, Canal Street  
 London SE1 7TL  
 www.feildenfowles.co.uk  
 Tel: 0203 7333 4566  
 info@feildenfowles.co.uk  
 © Feilden Fowles

Client:  
**National Railway Museum**  
 Site Address:  
 Leeman Rd, York YO26 4XU

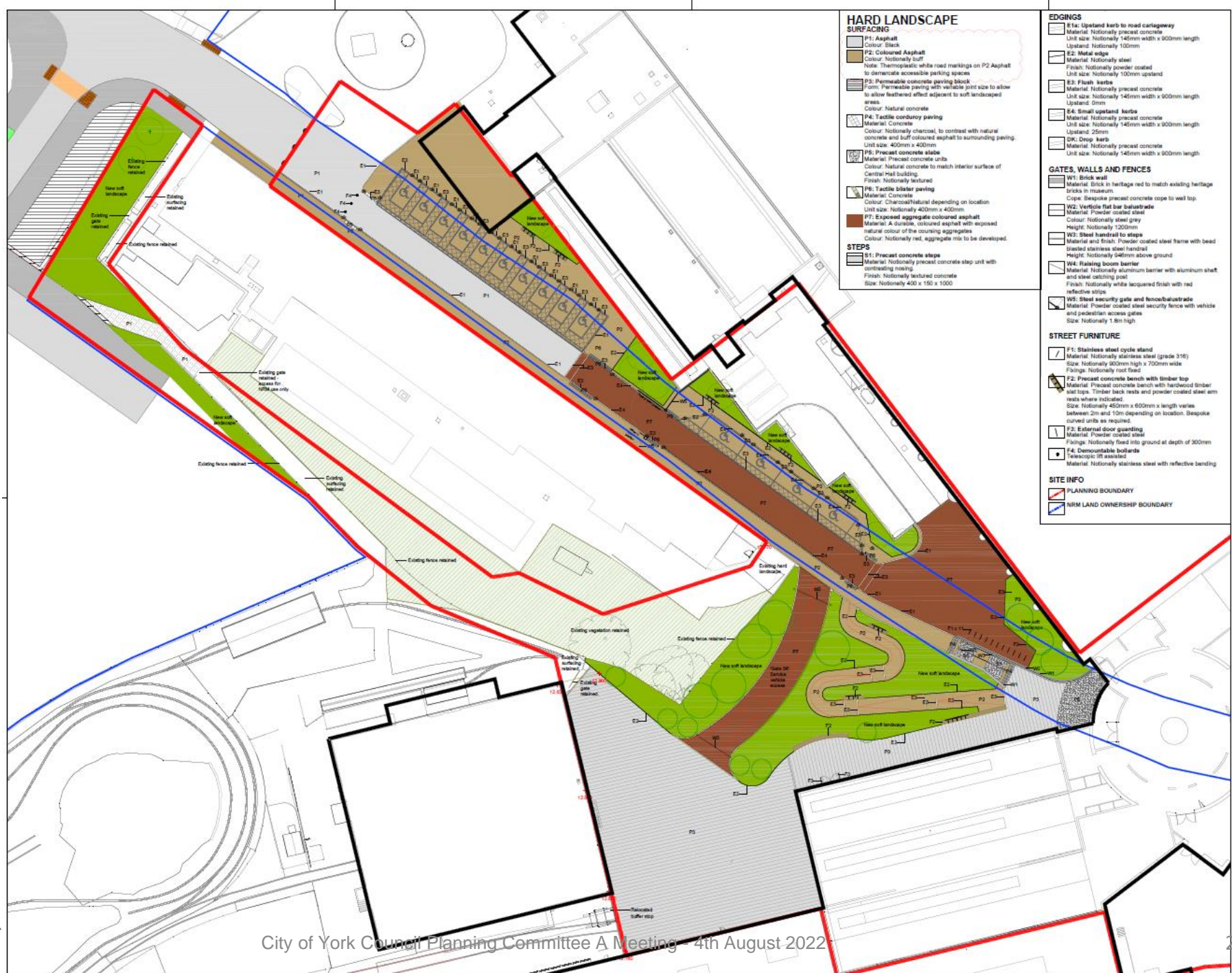
Project:  
**116 / CHY - Central Hall**

Drawing Title:  
**Proposed Long Section**

Project Name: <b>Stage 3</b>	Date: <b>20/12/21</b>
Scale @ A1: <b>1 : 500</b>	Scale @ A1: <b>1 : 250</b>
Client: <b>FFA</b>	Drawn: <b>FFA</b>
Drawing Number: <b>NRM-FFA-CH-XX-DR-A-03103</b>	Sheet Number: <b>P1-1</b>



# Proposed Hard Landscaping



**NOTES**

- This drawing is to be read in conjunction with all other drawings and specifications.
- Do not scale off this drawing. Within dimensions to be taken from this drawing.
- Any discrepancies found between this drawing and other drawings and specifications in the construction documents must be referred to the Landscape Architect prior to work commencing.
- This drawing must not be copied in whole or in part without prior written consent of Optimised Environments Ltd.

Issue	Revision	Author	Date
01	01	HW	17/02/22
02	01	HW	17/02/22
03	01	HW	17/02/22
04	01	HW	17/02/22

**open**  
optimised environments

Client: NATIONAL RAILWAY MUSEUM  
Project: NATIONAL RAILWAY MUSEUM YORK  
Drawing Title: LEEMAN ROAD / CENTRAL HALL GENERAL ARRANGEMENT DRAWING HARD LANDSCAPE

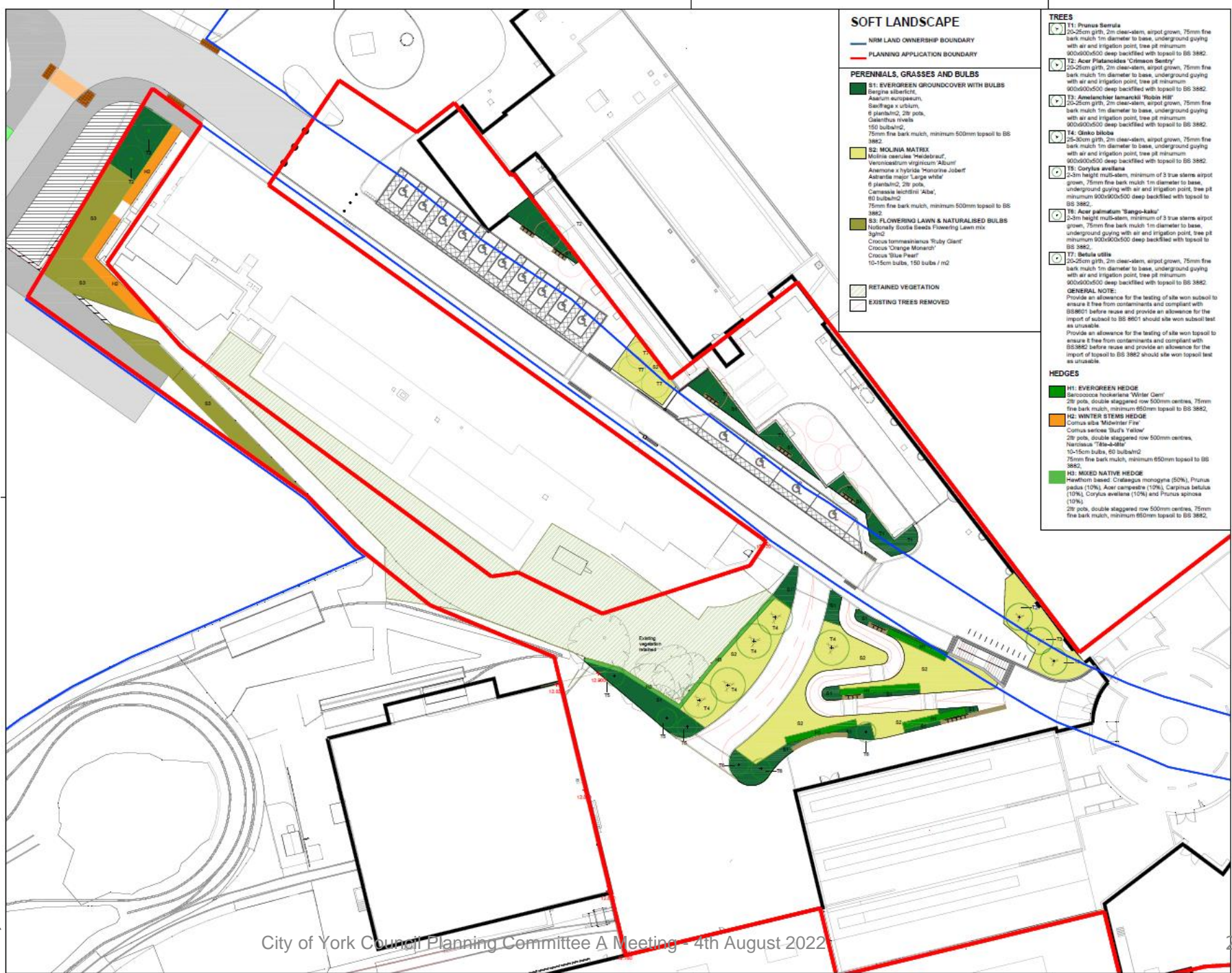
Scale Bar: 0m 2.5m 5.0m 7.5m 10m 12.5m  
Scale: 1:250@A1 Date: February 2022  
By: HW Status: PLANNING  
Checked: All Approved: SP

Drawing Number: 201564\_NRM\_OP\_SW\_0002  
Rev: 04





# Proposed Soft Landscaping



## SOFT LANDSCAPE

— NRM LAND OWNERSHIP BOUNDARY  
 — PLANNING APPLICATION BOUNDARY

### PERENNIALS, GRASSES AND BULBS

- S1: EVERGREEN GROUNDCOVER WITH BULBS**  
*Bergia albiflora*,  
*Aspidum nidulaeformis*,  
*Saxifraga x urbium*,  
 8 plants/m<sup>2</sup>, 28 pots,  
*Galanthus nivalis*  
 150 bulbs/m<sup>2</sup>,  
 75mm fine bark mulch, minimum 500mm topsoil to BS 3882
- S2: MOLINIA MATRIX**  
*Molinia caerulea* 'Heldraai',  
*Vernoniastrum virginicum* 'Albus',  
*Anemone x hybrida* 'Honorine Jobert',  
*Asterida major* 'Large white',  
 6 plants/m<sup>2</sup>, 28 pots,  
*Cernasea lactiflora* 'Alba',  
 60 bulbs/m<sup>2</sup>,  
 75mm fine bark mulch, minimum 500mm topsoil to BS 3882
- S3: FLOWERING LAWN & NATURALISED BULBS**  
*Nobolium* 'Scotia Seeds Flowering Lawn mix',  
 3/m<sup>2</sup>,  
*Crocus tommasinianus* 'Ruby Giant',  
*Crocus* 'Orange Monarch',  
*Crocus* 'Blue Pearl',  
 10-15cm bulbs, 150 bulbs / m<sup>2</sup>

▨ RETAINED VEGETATION  
 □ EXISTING TREES REMOVED

### TREES

- T1: Prunus Serrata**  
 200-25cm girth, 2m clear-stem, airpot grown, 75mm fine bark mulch 1m diameter to base, underground guying with air and irrigation point, tree pit minimum 900/600/500 deep backfilled with topsoil to BS 3882
- T2: Acer Platanoides 'Crimson Sentry'**  
 20-25cm girth, 2m clear-stem, airpot grown, 75mm fine bark mulch 1m diameter to base, underground guying with air and irrigation point, tree pit minimum 900/600/500 deep backfilled with topsoil to BS 3882
- T3: Amelanchier lamarckii 'Woburn Hill'**  
 20-25cm girth, 2m clear-stem, airpot grown, 75mm fine bark mulch 1m diameter to base, underground guying with air and irrigation point, tree pit minimum 900/600/500 deep backfilled with topsoil to BS 3882
- T4: Quercus ilex**  
 20-25cm girth, 2m clear-stem, airpot grown, 75mm fine bark mulch 1m diameter to base, underground guying with air and irrigation point, tree pit minimum 900/600/500 deep backfilled with topsoil to BS 3882
- T5: Corylus avellana**  
 2-3m height multi-stem, minimum of 3 true stems airpot grown, 75mm fine bark mulch 1m diameter to base, underground guying with air and irrigation point, tree pit minimum 900/600/500 deep backfilled with topsoil to BS 3882
- T6: Acer palmatum 'Sango-kaku'**  
 2-3m height multi-stem, minimum of 3 true stems airpot grown, 75mm fine bark mulch 1m diameter to base, underground guying with air and irrigation point, tree pit minimum 900/600/500 deep backfilled with topsoil to BS 3882
- T7: Betula utilis**  
 200-25cm girth, 2m clear-stem, airpot grown, 75mm fine bark mulch 1m diameter to base, underground guying with air and irrigation point, tree pit minimum 900/600/500 deep backfilled with topsoil to BS 3882

### HEDGES

- H1: EVERGREEN HEDGE**  
*Sarcococca hookeriana* 'Winter Gem',  
 28 pots, double staggered row 500mm centres, 75mm fine bark mulch, minimum 650mm topsoil to BS 3882
- H2: WINTER STEMS HEDGE**  
*Cornus alba* 'Midwinter Fire',  
*Cornus sericea* 'Sud's Yellow',  
 28 pots, double staggered row 500mm centres,  
*Narcissus* 'Tara-à-la',  
 10-15cm bulbs, 60 bulbs/m<sup>2</sup>,  
 75mm fine bark mulch, minimum 650mm topsoil to BS 3882
- H3: MIXED NATIVE HEDGE**  
 Hawthorn seeded *Crataegus monogyna* (50%), *Prunus padus* (10%), *Acer campestre* (10%), *Carpinus betulus* (10%), *Corylus avellana* (10%) and *Prunus spinosa* (10%),  
 28 pots, double staggered row 500mm centres, 75mm fine bark mulch, minimum 650mm topsoil to BS 3882

**NOTES**

- This drawing is to be read in conjunction with all other drawings and specifications.
- Do not scale off this drawing. Within dimensions to be taken off.
- Any discrepancies found between this drawing and other drawings and specifications in the associated documents must be referred to the Landscape Architect prior to work commencing.
- This drawing must not be copied in whole or in part without prior written consent of Openland Environments Ltd.

Rev	Description	By	Date
01	Issue for tender	AW	08/02/22
02	Revised drawing	AW	27/02/22
03	Revised drawing	AW	17/03/22
04	Revised drawing	AW	17/03/22

**open**  
 coordinated environments

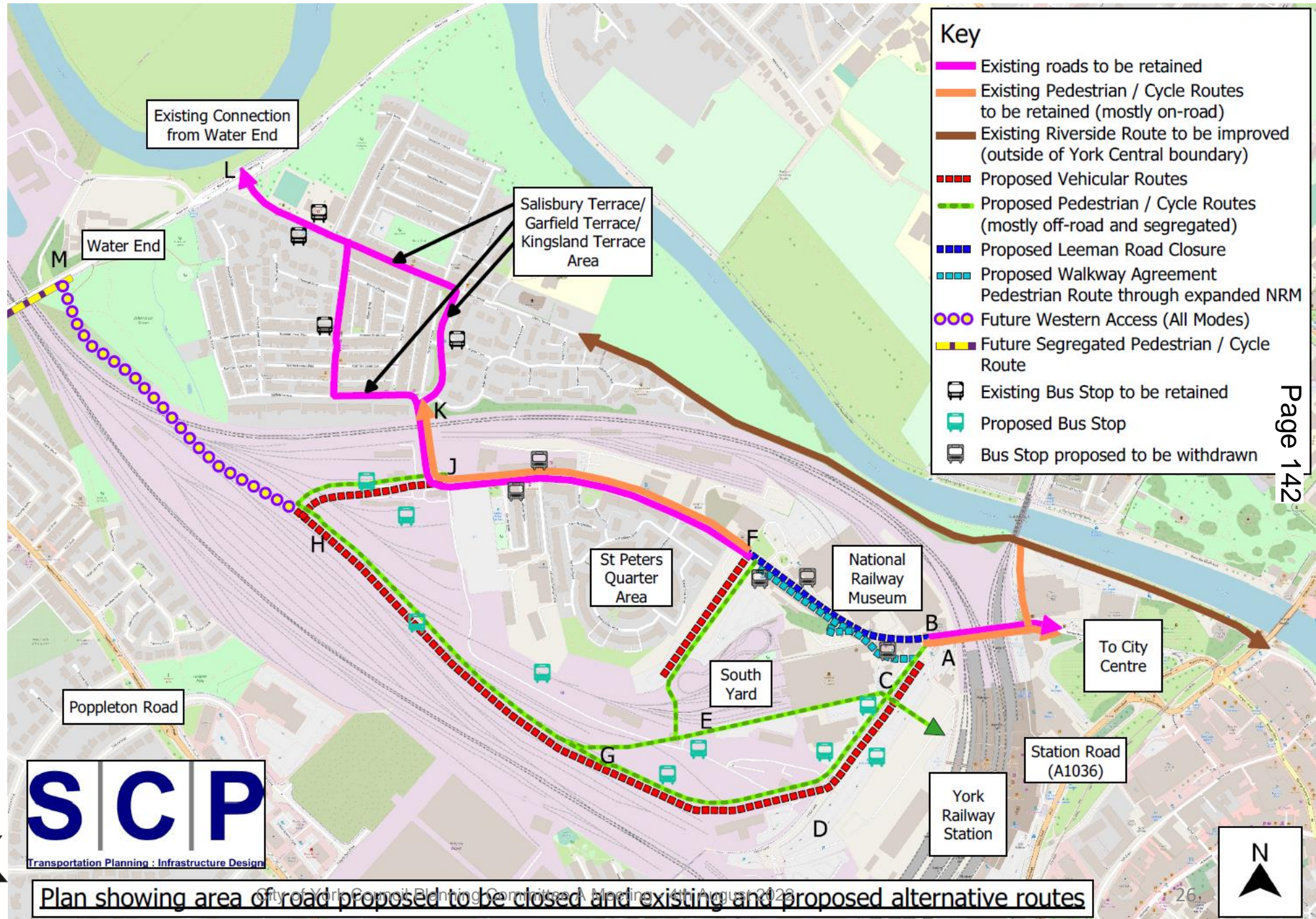
Client: NATIONAL RAILWAY MUSEUM  
 Project: NATIONAL RAILWAY MUSEUM YORK  
 Drawing Title: LEEMAN ROAD / CENTRAL HALL GENERAL ARRANGEMENT DRAWING SOFT LANDSCAPE

Scale: 1:250@A1  
 Date: February 2022  
 By: HW Status: PLANNING  
 Checked: AW Approved: SP

Drawing Number: 201564\_NRM\_OP\_SW\_0003  
 Plot: 04



# Plan Showing Alternative Routes



Plan showing area of road proposed to be closed and existing and proposed alternative routes

City of York Council Planning Committee A Meeting 1st August 2023

# Extent of Museum Square



- NOTES**
1. This drawing is to be read in conjunction with all other drawings and specifications.
  2. On the scale of this drawing. Within dimensions to be shown only.
  3. Any discrepancies found between this drawing and other drawings and specifications in the construction documents must be referred to the Landscape Architect prior to work commencing.
  4. This drawing must not be copied in whole or in part without prior written consent of Optimised Environments Ltd.

**SITE INFO**

	CENTRAL HALL PLANNING BOUNDARY
	NEW LAND OWNERSHIP BOUNDARY
	MUSEUM SQUARE RESA BOUNDARY (Reserved Matters Application Boundary)

Issue	Revision	Initial	Date

**open**  
Optimised Environments

Optimised Environments Ltd  
100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000

Client	NATIONAL RAILWAY MUSEUM
Project	NATIONAL RAILWAY MUSEUM YORK

Drawing Title	MUSEUM SQUARE RESERVED MATTERS APPLICATION BOUNDARY
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Scale Bar	North
0m 5.0m 10m 15.0m	
Scale: 1:250@A1	Date: MARCH 2022
By: AH	Status: PLANNING
Checked: AH	Approved: SP
Drawing Number	Rev
201564_NRM_OP_SW_0006	01



Visualisation of  
Museum Square



Page 144



Visualisation of  
Approach from  
Marble Arch



# Visualisation of Northern Approach



# Visualisation of the Internal of Central Hall



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**COMMITTEE REPORT**

**Date:** 4 August 2022      **Ward:** Holgate  
**Team:** West Area      **Parish:** Holgate Planning Panel

**Reference:** 22/00304/FULM  
**Application at:** Oak Haven 144 Acomb Road York YO24 4HA  
**For:** Erection of 64 bedroom residential care home (use class C2) with associated structures, access, parking and landscaping following demolition of existing structures  
**By:** St Marys (North Yorkshire) Ltd  
**Application Type:** Major Full Application  
**Target Date:** 11 July 2022  
**Recommendation:** Approve

**1.0 PROPOSAL**

1.1 Oak Haven, Acomb Road, Acomb comprises a disused two storey brick built care home dating to the late 1960s with a frontage on to York Road to the north east of the Acomb District Centre. The site provided 34 bed spaces for the care of the frail elderly before being closed in 2016 as part of a wider re-organisation and modernisation of provision for the elderly in the City. The existing site was felt not to be capable of being brought up to modern standards particularly in terms of provision of en-suite rooms. The building has been vacant since closure.

1.2 The site lies within a mixed use area at the fringe of the District centre with residential development to the north and to the south east with retail units within a former cinema to the west and a police station and GP practice/pharmacy to the east. Acomb Road is a major thoroughfare and public transport route giving access to and from the City Centre. The site comprises previously developed land and is a housing allocation in the Publication Draft Local Plan Policy H1 (Allocation Ref H 20) with a notional quantum of 56 units.

1.3 Planning permission is sought for the erection of a three storey brick built 64 bedroom care home with a pitched roof with the existing buildings completely demolished. The existing footprint would be broadly followed. The design has been derived from a lengthy period of negotiation.

## 2.0 POLICY CONTEXT

### NATIONAL PLANNING POLICY FRAMEWORK

2.1 The revised National Planning Policy Framework (NPPF) 2021 sets out the government's planning policies for England and how these are expected to be applied. It is a material consideration in the determination of this planning application.

2.2 Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise (section 38(6) Planning and Compulsory Purchase Act 2004).

2.3 The Statutory Development Plan for the City of York comprises the saved policies and key diagram of the otherwise revoked Yorkshire and Humber Plan Regional Spatial Strategy (2008) and any made Neighbourhood Plan.

### PUBLICATION DRAFT LOCAL PLAN (DLP 2018)

2.4 The DLP 2018 was submitted for examination on 25th May 2018. Phase 1 of the hearings into the examination of the Local Plan took place in December 2019 and consultation on proposed modifications to the plan were consulted on in line with Regulation 19 in 2019 and 2021. Phases 2 and 3 of the hearings took place in early 2022 with phase 4 scheduled later in the year. In accordance with paragraph 48 of the NPPF the DLP 2018 policies can be afforded weight according to:

-The stage of preparation of the emerging plan (the more advanced the preparation the greater the weight that may be given);

- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and

### 2.5 Publication Draft City of York Local Plan (2018) Policies

H1 Housing Allocations

HW7 Healthy Places

D1 – Place Making

D2 – Landscape and Setting-

D6 – Archaeology

GI2 - Biodiversity and access to nature

CC1 – Renewable and Low Carbon Energy Generation and Storage

CC2 – Sustainable Design and Construction of New Development

ENV2 Managing Environmental Quality

ENV3 – Land Contamination

NV5 – Sustainable Drainage

WM1- Sustainable waste management

T1 – Sustainable Access

T7 – Minimising and Accommodating Generated Trips

## 2.6 Emerging Local Plan Evidence Base

The evidence base that underpins the proposed emerging policies is considered to be a material consideration in the determination of this planning application. The directly relevant evidence base is

- 2014 City of York Strategic Housing Market Assessment (SHMA)
- 2016 City of York Strategic Housing Market Assessment (SHMA) (Addendum)

## DRAFT LOCAL PLAN 2005

2.7 The City of York Draft Local Plan incorporating the Fourth Set of Changes Development Control Local Plan (April 2005) was approved for Development Management purposes. The 2005 plan does not form part of the statutory development plan for the purposes of S38 (6) of the Planning and Compulsory Purchase Act 2004. Its policies are however considered capable of being material considerations in the determination of planning application where policies relevant to the application are consistent with those in the NPPF although the weight that can be attached to them is very limited.

## 3.0 CONSULTATIONS

INTERNAL

Design ,Conservation and Sustainable Development (Archaeologist)

3.1 Raise no objection to the proposal subject to any permission being condition to require the undertaking of a detailed evaluation prior to construction work being undertaken.

Design, Conservation and Sustainable Development (Landscape Architect)

3.2 Raise no objection to the proposal subject to any permission being conditioned to require the submission and approval of a detailed landscape scheme with a separate condition covering boundary treatments.

Design, Conservation and Sustainable Development (Ecologist)

3.3 Raise no objection in principle to the proposal subject to any permission being conditioned to safeguard nesting birds, biodiversity gain, the submission of landscape and environmental management plan and a lighting plan.

Public Protection

3.4 Raise no objection in principle to the proposal subject to any permission being conditioned in detail in respect of remediation of contaminated land, details of audible plant, details of odour dispersal apparatus, details of lighting, electric vehicle charging, hours of construction and a Construction Environmental Management Plan (CEMP).

Highway Network Management

3.5 Raise no objection in principle to the proposal subject to any permission being conditioned in detail in respect of site layout and parking.

Flood Risk Management

3.6 Any response will be reported verbally.

EXTERNAL

Holgate Planning Panel

3.7 No response received.

Ainsty (2008) Internal Drainage Board

3.8 Raise no objection in principle to the proposal subject to any permission being conditioned to require the submission and prior approval of a detailed surface water drainage scheme.

Yorkshire Water Services Limited

3.9 Raise no objection in principle to the proposal subject to any permission being conditioned to require submission and prior approval of a detailed surface water drainage scheme for the site.

## **4.0 REPRESENTATIONS**

Neighbour Notification and Publicity

4.1 Two letters of objection have been received and two of support. The letters of objection raise the following issues:

- Objection to the loss of trees of townscape importance

4.2 The letters of support raise the following issues:

- Support for the scheme subject to the existing fruit trees on the site being retained and the planting enhanced.
- Support for the scheme subject to adequate measures being put in place to minimise parking in the surrounding area.

## **5.0 APPRAISAL**

### **5.1 KEY CONSIDERATIONS INCLUDE**

- Principle of the Development
- Need for older persons accommodation
- Design and layout
- Trees and Landscape
- Highways and Access
- Drainage and Flood Risk

- Residential amenity
- Sustainability

## PRINCIPLE OF THE DEVELOPMENT

5.2 Central Government Planning Policy as outlined in paragraph 119 of the NPPF indicates that planning decisions should promote the effective use of land in meeting the need for homes and other uses. In addition paragraph 120 indicates that substantial weight should be given to the use of brownfield land within settlements for homes and other identified needs.

5.3 The application site comprises a disused care home constructed in the late 1960s set within a densely developed area within the inner urban area of the City directly to the east of the Acomb District Centre. It is highly prominent in the wider street scene adjacent to the principal route between the Acomb District Centre and the City Centre and its deteriorating physical condition is a detractor to the visual amenity of the wider street scene.

5.4 The existing building has previously been identified as not being readily capable of refurbishment to meet modern needs and so it is proposed to demolish the existing structure and re-develop the cleared site. Central Government planning policy in respect of making the most efficient use of land proactively encourages the use of brownfield land such as the application site and the site is also very sustainably located with shops and other services such as a doctor's surgery and pharmacy very close by. The development furthermore provides for a critical need in terms of provision for elderly care as will be outlined below. With the existing use of the site as a care home the principle of the development of the site is therefore felt to be acceptable notwithstanding the sustainability impact of demolition. The impact of the proposal on the Acomb District Centre would be broadly neutral.

## NEED FOR OLDER PERSONS ACCOMMODATION

5.5 Central Government Planning Policy as outlined in Section 5 of the NPPF specifically paragraph 62 indicates that the size, type and tenure of housing needed for different community groups should be assessed and clearly reflected in detailed planning policy including for older people. The NPPG identifies a doubling of the very elderly population aged 85 and over by 2041 with an increasingly critical need for accommodation to meet the specialist needs of the age group. Research suggests that 30% of elderly accommodation does not reach modern standards.

5.6 In terms of need within the City figures provided by Adult Social Care indicate in 2019 a demand for 2034 places with a supply including projects under construction of 1520 places giving a shortfall of 514 places. This short fall would rise to 1614 places by 2039 without significant new investment. A report to the Adult Social Care Scrutiny Committee in 2015 identified historic issues of under investment with 225 beds in the Local Authority sector no longer fit for purpose primarily in terms of not being en suite which has been a requirement in terms of new build operations since 2002. At the same time the number of residents within the City aged 75 and over are expected to increase by the order of 50% from 17,200 to 25,800 over the next 15 years. National Adult Social Care benchmarks indicate that for every 100 residents over 75 years of age there should be 11 care bed spaces available. Current data indicates an occupancy rate of 98% in terms of existing provision within the City.

5.7 The application site comprises a former Local Authority Care Home that closed in 2016. It is included in Policy H1 of the Publication Draft Local Plan as a draft allocation ref H20 giving a hypothecated yield of 56 units with the previous care home catering for 34 residents. The new proposal envisages the construction of specialist supported housing for older people which would be regulated by the CQC (Care Quality Commission). This in turn is supported by Policy H9 of the Publication Draft Local Plan with the written explanation for the Policy indicating that the City has an above average number of elderly residents with a high proportion of those aged over 85 years. That age group is furthermore particularly vulnerable to a number of health issues including dementia and mobility problems.

5.8 The City of York Strategic Housing Market Assessment(SHMA) and 2016 Addendum indicate that there is an identified additional need of 37 bed spaces for those aged 75 and over per annum over the period 2012 to 2033 with an emphasis on specialist provision for those with dementia which the proposal would make a significant contribution towards fulfilling. The site includes an area of resident's garden and is sustainably located in terms of access to local amenities including a pharmacy and several doctor's surgeries. It is therefore felt that the proposal would make a significant contribution to meeting identified need for specialist elderly care beds in a sustainable location.

## DESIGN AND LAYOUT

5.9 Central Government Planning Policy as outlined at paragraph 130 a) b) and c) indicates that planning decisions should create developments which function well and add to the overall quality of the area, are visually attractive as a result of good

architecture, layout and appropriate landscaping and are sympathetic to local character including the surrounding built environment and landscape setting. Policy D1 of the Publication Draft Local Plan indicates that development proposals should enhance and respect the surrounding pattern of street blocks, plots and buildings whilst demonstrating that the resulting pattern of density will be appropriate for the proposed use and surrounding context whilst demonstrating that the combined effect of development would not over-dominate surrounding buildings.

5.10 The application site whilst not being within a Conservation Area or within the setting of any other Designated Heritage Assets is highly prominent within the wider street scene defining the character of the principal approach between the City Centre to the east and the Acomb District Centre to the west. The existing building is comparatively low rise and set back from the street frontage with an area of green space to the front. To the west is a substantial brick built former cinema dating to the early 1930s now converted into small retail units with a police station of more recent construction close up to the road frontage to the east. To the south west on the opposite side of Acomb Road is a dense pattern retail units and other town centre fringe type uses located within former dwellings. The existing building is out of keeping with the properties to either side within the street frontage appearing diminutive in scale. Furthermore its deteriorating physical condition detracts from the visual amenity of the wider street scene.

5.11 The design of the proposal has been extensively developed and refined in order to ensure that it fits in better with the wider context. The design has been broken up in order to lessen its scale and massing but at the same time better relate the development to its immediate surroundings; with the use of an under-croft, the introduction of mini-gables, setting longitudinal sections of roof behind low brick parapets along with bringing elements of the Acomb Road elevation forward with some being physically higher. Small but significant areas of new landscape planting have been incorporated on to the street frontage as well as a series of glazed amenity areas, some at a higher level for residents along with a partial section of green wall. The rear elevation meanwhile is set back and set down from the boundary with properties in Hebden Rise and Baildon Close with small glazed balcony external amenity areas for residents with prominent brick clad gable features the roof lowered behind a brick parapet.

5.12 Overall it is felt that the proposed structure would better respect its immediate surroundings relative to the existing building and the previous forms of the current design. The proposed palette of materials with use of mid red brick and slate together with some use of structural glazing clearly references that of surrounding



buildings within the Acomb Road frontage and better respects and enhances the contribution of the site to the wider visual amenity of the street scene.

## TREES AND LANDSCAPE

5.13 Central Government Planning Policy as outlined in paragraph 131 of the NPPF indicates that planning decisions should ensure that opportunities are taken to incorporate trees elsewhere in developments that appropriate measures are secured to ensure the long term maintenance of newly planted trees and that existing trees are retained wherever possible. Policy G14 of the Publication Draft Local Plan indicates that development proposals will be supported where trees and hedgerows that contribute to the setting of a proposed development are retained.

5.14 The application site includes substantial areas of tree planting including a fastigiate oak on the site frontage together with other areas of semi-mature shrub and tree planting. To the rear facing Baildon Close is a significant area of semi-mature fruit trees mainly cherries which the submitted tree survey identifies as being in fair condition. These provide some degree of amenity to the communal gardens of the properties to the north in Baildon Close but are less readily visible from Hebden Rise to the north-west. The area would largely be cleared to allow for the creation of the staff and visitor parking area proposed to the rear of the site. The scheme however proposes the addition of significant additional rear boundary planting along the boundary of the site with Baildon Close along with significant additional planting long the Acomb Road frontage to complement the retention of the fastigiate Oak thereby enhancing the resident's external amenity area. The proposals are therefore felt on balance to be acceptable.

## HIGHWAYS AND ACCESS

5.15 Central Government Planning Policy as outlined in paragraph 111 of the NPPF indicates that development should only be refused or prevented on highway grounds where there would be an unacceptable impact upon highway safety or the residual cumulative impact upon the road network would be severe. Policy T1 of the Publication Draft Local Plan indicates that development will be supported where it minimises the need to travel and ensures safe and appropriate access to the adjoining highway, there are safe and appropriate links to local services and facilities and there is sufficient convenient and secure cycle parking within the site.

5.16 The existing site had only minimal vehicle parking and no formal cycle parking within it with the result that previously staff parked largely in the adjoining side streets. Concern has been expressed by neighbours in respect of a return to this scenario particularly with the greater degree of intensity of the new development. A

vehicle parking area of 19 spaces (including two disabled) for staff and visitors would be provided to the north of the site. Associated with this would be a secure bin store and cycle parking area providing for 12 spaces. A layby would be provided for refuse collection again accessed from Hebdon Rise. The proposed provision is in accordance with parking guidelines and subject to any permission being appropriately conditioned then the proposal is felt to be acceptable in highway terms.

## DRAINAGE AND FLOOD RISK

5.17 Central Government Planning Policy as outlined in paragraph 167 of the NPPF indicates that in determining planning applications Local Planning Authorities should ensure that flood risk is not increased elsewhere. At the same time Policy ENV5 of the Publication Draft Local Plan indicates that in respect of brownfield sites surface water drainage should be restricted to 70% of the existing run-off rate unless it can be demonstrated to be impractical. Sufficient storage capacity should be provided to allow for the impact of a 1 in 30 year storm event without the run off rate being exceeded.

5.18 The application site is in Flood Zone 1 and therefore felt to be at low risk of flooding. A detailed surface water drainage strategy has been submitted which establishes that the site can be safely drained with a reduced run off rate allowing for the impacts of climate change.

5.19 Precise details of the surface water drainage scheme can be secured by condition on any permission.

## RESIDENTIAL AMENITY

5.20 Central Government Planning Policy as outlined in paragraph 130f) of the NPPF indicates that planning decisions should create places with a high standard of amenity for existing and future users. At the same time Policy ENV2 of the Publication Draft City of York Publication Draft Local Plan indicates that development proposals that are likely to have an impact upon the amenity of the surrounding area including residential amenity should be accompanied by evidence that the impacts have been evaluated and that there would not be any resulting loss of character or amenity.

5.21 The application site comprises the site of a former Care Home set within a densely developed built frontage directly to the east of the Acomb District Centre. Within the adjoining frontage are a mix of town centre fringe type uses including a police station, a doctor's surgery and specialist retail uses. Residential development

comprising flats lies to the north and east in Hebden Rise and Baildon Close with the gardens associated with Baildon Close overlooking the rear of the site. Presently these are planted with a range of orchard but mainly cherry trees which provide a degree of amenity to those using the gardens. The area is however to be developed to form the required off-street vehicle and cycle parking area for the development leading to a loss of amenity. This would however to an extent be balanced by proposed reinforcement planting at the site boundary.

5.22 The properties in Baildon Close are elevated relative to the application site with gables directly facing the rear of the proposed development with a minimum distance of approximately 20 metres. There would not be any significant overlooking between the two groups of properties. A detailed sun path analysis has been submitted as part of the Design and Access Statement which identifies that there would be no significant loss of daylight or sunlight to the adjoining properties.

5.23 In terms of the amenities of prospective residents the rooms and communal facilities comply with the Nationally Prescribed Space Standards. A garden area would be provided for those residents able to use it on the main Acomb Road frontage. A series of smaller balconied terrace spaces would be provided at first floor level on front and rear elevations for the use of those residents unable to access the outside.

5.24 Notwithstanding the loss of a degree of amenity to the gardens of the properties in Baildon Close through the significant reduction in the existing tree cover the development is felt to be acceptable in terms of its impact upon the residential amenity of neighbouring properties and the amenities of prospective occupants of the development.

## SUSTAINABILITY

5.26 The requirements of policy CC1 of the Publication Draft Local Plan in terms of carbon reduction have been largely superseded by Part L of the 2021 edition of the Building Regulations.

5.27 Detailed sustainability information has been submitted as part of the Design and Access Statement. This indicates that issues of solar gain will be address through the orientation of the new building together with window size and placement relative to the internal layout. Building materials will be sourced from low carbon sources with the use of recycling wherever possible. Only sustainably sourced timber will be used for windows and internal fixtures and fittings. The energy reduction targets of Policies CC1 and CC2 would be met through energy efficiency measures. Scope is also identified for use of both roof mounted solar pvs and also

CHP (Combined Heat and Power) plant. Sustainability issues including water usage will be addressed by condition as part of any decision.

## 6.0 CONCLUSION

6.1 Oak Haven comprises a disused brick built care home dating to the late 1960s with a substantial frontage on to York Road to the north east of the Acomb District Centre. Planning permission is sought for the erection of a three storey brick built 64 bedroom care home with a pitched roof following on from the demolition of the existing structures on site. The existing built footprint would be broadly followed in terms of the new construction. It is felt that the proposal would provide much needed specialist elderly residential care to part remedy existing deficiencies. It would provide a sensitive design solution for a visually sensitive location in street scene terms. It would make appropriate use of landscaping both for the amenity of residents and the amenity of the wider area and it seeks to minimise parking off site in the surrounding area. The proposal is therefore felt to comply with the policies of the NPPF and the Publication Draft Local Plan 2018.

## 7.0 RECOMMENDATION: Approve

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans:-

Drawing Refs:;4463-WRD-XX-ZZ-DR-A-0080 P2; 4463-WRD-XX-ZZ-DR-A-0500 P11 ; 21-373-TR-003 ; WRD1003-001 ; 5812-DR-01 P2 ; 4463-WRD-XX-ZZ-DR-A-0300\_P02 ; 4463-WRD-XX-B1-DR-A-0200 P4; 4463-WRD-XX-03-DR-A-0204 P1 ; 4463-WRD-XX-02-DR-A-0203 P5 ; 4463-WRD-XX-01-DR-A-0202 P5 ; 4463-WRD-XX-00-DR-A-0201 P4.

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 Notwithstanding any proposed materials specified on the approved drawings or in the application form submitted with the application, samples of the external materials to be used shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the construction of the development beyond foundation level. The development shall be carried out using the approved materials.

Note: sample materials should be made available for inspection at the site. Please make it clear in your approval of details application when the materials will be available for inspection and where they are located.

Reason: So as to achieve a visually cohesive appearance.

4 Details of all means of enclosure to the site boundaries shall be submitted to and approved in writing by the Local Planning Authority before the construction of the development commences beyond foundation level and shall be provided in accordance with the approved details before the development is occupied.

Reason: In the interests of the visual amenities of the area and the amenities of neighbouring properties.

5 Prior to the commencement of construction works details of the proposed means of foul and surface water drainage, including shall be submitted to and approved in writing by the Local Planning Authority, including

- a) Evidence that surface water drainage by infiltration will not be practical;
- b) Evidence of existing positive drainage to public sewer and the points of connection;
- c) The means of restricting discharge to the public sewer to the existing rate less a minimum 30% reduction based upon the existing peak discharge during a 1 in 1 year storm event to allow for climate change

and the development shall be carried out in accordance with the approved details.

The site shall be developed with separate systems of foul and surface water drainage on and off site. The separate systems shall extend to the points of discharge to be agreed.

There shall be no piped discharge of surface water from the development site prior to the completion of surface water drainage.

Reason: So that the Local Planning Authority may be satisfied with these details for the proper and sustainable drainage of the site.

6 A programme of post-determination archaeological evaluation is required on this site. The archaeological scheme comprises 3-5 stages of work. Each stage shall be completed and agreed by the Local Planning Authority (LPA) before it can be approved.

A) No archaeological evaluation or development shall take place until a written scheme of investigation (WSI) for archaeological evaluation has been submitted to and approved by the local planning authority in writing. The WSI should conform to standards set by Local Planning Authority and the Chartered Institute for Archaeologists.

B) The site investigation and post investigation assessment shall be completed in accordance with the programme set out in the Written Scheme of Investigation approved under condition (A) and the provision made for analysis, publication and dissemination of results and archive deposition will be secured. This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI.

C) A copy of a report on the evaluation and an assessment of the impact of the proposed development on any of the archaeological remains identified in the evaluation shall be deposited with City of York Historic Environment Record to allow public dissemination of results within 6 weeks of completion or such other period as may be agreed in writing with the Local Planning Authority.

D) Where archaeological features and deposits are identified proposals for the preservation in-situ, or for the investigation, recording and recovery of archaeological remains and the publishing of findings shall be submitted as an amendment to the original WSI. There shall be presumption in favour of preservation in-situ wherever feasible.

E) No development shall take place until:

- details in D have been approved and implemented on site

- provision has been made for analysis, dissemination of results and archive deposition has been secured

- a copy of a report on the archaeological works detailed in Part D should be deposited with City of York Historic Environment Record within 3 months of completion or such other period as may be agreed in writing with the Local Planning Authority.

Reason: The site lies within an area of archaeological interest. An investigation is required to identify the presence and significance of archaeological features and deposits and ensure that archaeological features and deposits are recorded prior to destruction. This condition is imposed in accordance with Section 16 of NPPF.

7 Prior to commencement of the development, a Construction Environment Management Plan (CEMP) for minimising the creation of noise, vibration and dust during the demolition, site preparation and construction phases of the development

shall be submitted to and approved in writing by the Local Planning Authority. The CEMP must include a site specific risk assessment of dust impacts in line with the guidance provided by IAQM (see <http://iaqm.co.uk/guidance/>) and include a package of mitigation measures commensurate with the risk identified in the assessment. All works on site shall be undertaken in accordance with the approved scheme, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To protect the amenity of the locality

8 The hours of construction, loading or unloading on the site shall be confined to 8:00 to 18:00 Monday to Friday, 9:00 to 13:00 Saturday and no working on Sundays or public holidays.

Reason: To protect the amenities of adjacent residents.

9 Before the occupation of the development 1 Electric Vehicle Recharging Point(s) shall be provided in a position and to a specification to be first approved in writing by the Local Planning Authority (active provision). In addition, a minimum of 1 additional parking bays should be identified for the future installation of additional Electric Vehicle Charging Points. Such additional bays should be provided with all necessary ducting, cabling and groundwork to facilitate the addition of Electric Vehicle Charge Points in the future, if required (passive provision). Charging points should be located in a prominent position on the site and should be marked for the exclusive use of zero emission vehicles.

Reason: To promote and facilitate the uptake of electric vehicles on the site in line with the Council's Low Emission Strategy (LES) and the National Planning Policy Framework (NPPF).

10 Prior to development, an investigation and risk assessment (in addition to any assessment provided with the planning application) shall be undertaken to assess the nature and extent of any land contamination. The investigation and risk assessment must be undertaken by competent persons. A written report of the findings shall be produced, submitted to and approval in writing of the Local Planning Authority. The report of the findings must include:

- (i) a survey of the extent, scale and nature of contamination (including ground gases where appropriate);
- (ii) an assessment of the potential risks to:
  - human health,
  - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
  - adjoining land,
  - groundwaters and surface waters,
  - ecological systems,

- archaeological sites and ancient monuments;
- (iii) an appraisal of remedial options, and proposal of the preferred option(s).

This shall be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11'.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

11 Prior to development, a detailed remediation scheme to bring the site to a condition suitable for the intended use (by removing unacceptable risks to human health, buildings and other property and the natural and historical environment) shall be submitted to and approved in writing of the Local Planning Authority. The scheme shall include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme shall ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

12 Prior to first occupation or use, the approved remediation scheme shall be carried out in accordance with its terms and a verification report that demonstrates the effectiveness of the remediation carried out must be produced and is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems.

13 In the event that contamination is found at any time when carrying out the approved development that was not previously identified, it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken and where remediation is necessary a remediation scheme must be prepared, which is subject to the approval in writing of the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority.



Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

14 No development shall take place until a detailed scheme of noise insulation measures for protecting the approved residential from externally generated noise has been submitted to and approved in writing by the Local Planning Authority. Upon completion of the approved insulation scheme works no part of the development shall be occupied until a noise report demonstrating compliance with the approved noise insulation scheme has been submitted to and approved in writing by the Local Planning Authority.

INFORMATIVE: The building envelope of all residential accommodation shall be constructed so as to achieve internal noise levels in habitable rooms of no greater than 35 dB LAeq (16 hour) during the day (07:00-23:00 hrs) and 30 dB LAeq (8 hour) and LAFMax level during the night (23:00-07:00 hours) should not exceed 45dB(A) on more than 10 occasions in any night time period in bedrooms and should not regularly exceed 55dB(A). These noise levels shall be observed with all windows open in the habitable rooms or if necessary windows closed and other means of ventilation provided.

Reason: To protect the amenity of people living in the new property from externally generated noise and in accordance with the National Planning Policy Framework.

15 Details of all machinery, plant and equipment to be installed in or located on the premises, which is audible outside of the premises, shall be submitted to the local planning authority for approval. These details shall include average sound levels (LAeq), octave band noise levels and any proposed noise mitigation measures. The machinery, plant or equipment and any approved noise mitigation measures shall be fully implemented and operational before the proposed use first opens and shall be appropriately maintained thereafter.

Note: The combined rating level of any building service noise associated with plant or equipment at the site should not exceed the representative LA90 1 hour during the hours of 07:00 to 23:00 or representative LA90 15 minutes during the hours of 23:00 to 07:00 at 1 metre from the nearest noise sensitive facades when assessed in accordance with BS4142: 2014, inclusive of any acoustic feature corrections associated with tonal, impulsive, distinctive or intermittent characteristics.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

16 There shall be adequate facilities for the treatment and extraction of cooking

odours. Details of the extraction plant or machinery and any filtration system required shall be submitted to the local planning authority for written approval. It shall be installed and fully operational as approved before the proposed use first opens and shall be appropriately maintained and serviced thereafter in accordance with manufacturer guidelines.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

17 All external lighting, other than that required for emergency or security purposes, shall be turned off by 23:00 on any day.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area

18 No vegetation clearance, tree works or building demolition works shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a detailed check of suitable habitat for active birds' nests immediately before the start of works and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the local planning authority prior to any vegetation clearance, tree works or building demolition works commencing.

Reason: To ensure that breeding birds are protected from harm during construction. All British birds, their nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife and Countryside Act 1981, as amended.

19 A biodiversity enhancement plan/drawing shall be submitted to, and be approved in writing by, the local planning authority prior to the commencement of works. The content of the plan shall include the recommendations set-out in the Ecological Appraisal, Wold Ecology Ltd., July 2021, as already submitted with the planning application and agreed in principle with the local planning authority prior to determination. Development shall be carried out in accordance with the approved details.

Reason: To take account of and enhance the biodiversity and wildlife interest of the area, and to be in accordance with Paragraph 174 d) of the NPPF (2021) to contribute to and enhance the natural and local environment by minimising impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures.

20 Prior to the installation of any new external lighting, a 'lighting design plan' shall be submitted to and approved in writing by the local planning authority. The external lighting permitted shall be carried out in accordance with the approved

'lighting design plan'.

The plan shall:

Demonstrate that required external lighting has been selected in-line with current guidance - Bat Conservation Trust (2018) Bats and artificial lighting in the UK.  
<https://cdn.bats.org.uk/pdf/Resources/ilp-guidance-note-8-bats-and-artificial-lighting-compressed.pdf?mtime=20181113114229&focal=none>

Show how and where external lighting will be installed, so that it can be clearly demonstrated that areas to be lit will not disturb light-sensitive wildlife, such as bats.

Reason: To maintain the favourable conservation status of bats and ensure the site remains attractive to other light sensitive species.

21 In order to manage and maintain the ecological value of the application site a landscape and ecological management plan (LEMP) shall be submitted to, and be approved in writing by, the local planning authority prior to the commencement of the development. The content of the LEMP shall include the following:

- a) Description and evaluation of features to be managed.
- b) Ecological trends and constraints on site that might influence management.
- c) Aims and objectives of management.
- d) Appropriate management options for achieving aims and objectives.
- e) Prescriptions for management actions.
- f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period).
- g) Details of the body or organisation responsible for implementation of the plan.
- h) Ongoing monitoring and remedial measures.

The LEMP shall also include details of the legal and funding mechanism(s) by which the long-term implementation of the plan will be secured by the developer with the management body(ies) responsible for its delivery. The plan shall also set out (where the results from monitoring show that conservation aims and objectives of the LEMP are not being met) how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme.

The completed scheme shall be managed and/or maintained in accordance with the approved management plan.

Reason: To ensure wildlife mitigation, compensation and enhancements measure are managed and maintained appropriately.

22 Before the commencement of development (including demolition, excavations, and building operations), a complete and detailed Arboricultural Method Statement and scheme of arboricultural supervision regarding protection measures for existing trees within and adjacent to the application site shown to be retained on the approved drawings, shall be submitted to and approved in writing by the Local

Planning Authority. Amongst other information, this statement shall include details and locations of protective fencing, ground protection, a schedule of tree works if applicable, site rules and prohibitions, phasing of protection measures, site access during demolition/construction, types of construction machinery/vehicles to be used (including delivery and collection lorries and arrangements for loading/off-loading), specialist construction techniques where applicable, parking arrangements for site vehicles, locations for stored materials, and means of moving materials around the site, locations and means of installing utilities, and location of site compound. The document shall also include methodology and construction details and existing and proposed levels where a change in surface material and boundary treatments is proposed within the root protection area of existing trees. A copy of the document will be available for reference and inspection on site at all times.

Reason: To ensure every effort and reasonable duty of care is exercised during the development process in the interests of protecting the existing trees shown to be retained which are considered to make a significant contribution to the public amenity and setting of the development.

23 Within three months of commencement of development a detailed landscape scheme shall be submitted to and approved in writing by the Local Planning Authority. This shall include the species, stock size, density (spacing), and position of trees, shrubs and other plants. It will also include tree planting details. The plans and details shall illustrate that the tree planting is compatible with existing and proposed utilities. This scheme shall be implemented within a period of six months from the practical completion of the development. Any trees or plants which die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of a similar size and species, unless the Local Planning Authority agrees alternatives in writing.

Reason: So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of trees and shrubs across the site, since the landscape scheme, is integral to the amenity of the development and the immediate area, and forms part of the mitigation for trees lost as a result of the development.

24 A fully detailed drawing at a standard metric scale illustrating the design and materials of all footpaths and other adoptable open spaces shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of their construction on site. The development shall be carried out as approved.

Reason To ensure that the site access can be properly maintained and to secure compliance with Policy T1 of the Publication Draft Local Plan (2018)

25 Vehicular access to the site hereby authorised shall be from Hebden Rise and details of the design of this access, together with associated sightlines, shall be submitted to and approved in writing by the Local Planning Authority prior to the

commencement of the construction of the development. The development shall be carried out as approved.

Reason: To ensure that the site may be satisfactorily accessed and to secure compliance with Policy T1 of the Publication Draft Local Plan (2018)

26 The development hereby permitted shall not come into use until the following highway works have been carried out in accordance with details which shall have been previously submitted to and approved in writing by the Local Planning Authority, or arrangements entered into which ensure the same - Tactile paving to link the internal/external footpaths; 2m wide pedestrian marked footway along the front of the units linking into the pedestrian access at the west of the site; Give way markings where the access road meets Hebden Rise. The site shall be maintained as such thereafter.

Reason : To ensure that the site may be satisfactorily accessed and to secure compliance with Policy T1 of the Publication Draft City of York Local Plan (2018)

27 The development shall not come into use until the junction with the public highway has been constructed in accordance with the approved plans.

Reason: In the interests of road safety.

28 Prior to the commencement of the use hereby approved, provision shall be made within the site for accommodation of delivery/service vehicles in accordance with details which shall have been previously submitted to and approved in writing by the Local Planning Authority. Thereafter all such areas shall be retained free of all obstructions and used solely for the intended purpose.

Reason: To ensure that delivery/service vehicles can be accommodated within the site and to maintain the free and safe passage of highway users.

29 Prior to the commencement of the construction of the development details of the cycle parking area including a means of cover and enclosure, shall be submitted to and approved in writing by the Local Planning Authority. The development shall not be occupied until the cycle parking area has been provided in accordance with the approved details, and these areas shall not be used for any purpose other than the parking of cycles.

Reason: To promote use of cycles thereby reducing congestion on the adjacent roads and in the interests of the amenity of neighbours.

30 The building shall not be occupied until the areas shown on the approved plans for parking and manoeuvring of vehicles (and cycles, if shown) have been constructed and laid out in accordance with the approved plans, and thereafter such

areas shall be retained solely for such purposes.

Reason: In the interests of highway safety.

31 Prior to the development coming into use 2.0 x 2.0m sight lines, free of all obstructions which exceed the height of the adjacent footway by more than 0.6m, shall be provided both sides of the junction of any access with the footway, and shall thereafter be so maintained.

Reason: In the interests of pedestrian safety.

32 A detailed method of works statement identifying the programming and management of site clearance/preparatory and construction works shall be submitted to and approved in writing by the Local Planning Authority prior to the development commencing. The statement shall include at least the following information:

- measures to prevent the egress of mud and other detritus onto the adjacent public highway;
- the routing for construction traffic that will be promoted;
- a scheme for signing the promoted construction traffic routing;
- where contractors will park; and
- where materials will be stored within the site.

The scheme for the management of site clearance/preparatory and construction works shall be carried out as approved.

Reason: To secure the Amenity of the Surrounding Area and to secure compliance with Policy T1 of the 2018 Publication Draft Local Plan

33 The development hereby approved shall achieve a water consumption rate of 110 litres per person per day (calculated as per Part G of the Building Regulations).

Reason: To fulfil the environmental objectives of the NPPF and support the transition to a low carbon future, and in accordance with policies CC1 and CC2 of the Publication Draft Local Plan 2018.

## **8.0 INFORMATIVES:**

### **Notes to Applicant**

#### **1. STATEMENT OF THE COUNCIL`S POSITIVE AND PROACTIVE APPROACH**

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application.

The Local Planning Authority took the following steps in order to achieve a positive outcome:

- i) Sought amendment of the proposal to retain the fastigate oak on the Acomb Road frontage of the site together with the maximum level of planting on the rear site boundary.
- ii) Sought amendment of the internal layout to maximise vehicle and secure cycle parking to minimise off site parking in surrounding streets.

## 2. CONSENT FOR HIGHWAY WORKS:

You are advised that prior to starting on site consent will be required from the Highway Authority for the works being proposed, under the Highways Act 1980 (unless alternatively specified under the legislation or Regulations listed below). For further information please contact the officer named:

- Adoption of highway (Section 38) - [development.adoption@york.gov.uk](mailto:development.adoption@york.gov.uk)
- Agreements as to execution of works (Section 278) - [development.adoption@york.gov.uk](mailto:development.adoption@york.gov.uk)
- Planting in the highway (Section 142)
- Scaffolding licence (Section 169) [highway.regulation@york.gov.uk](mailto:highway.regulation@york.gov.uk)
- Works in the highway (Section 171) - [streetworks@york.gov.uk](mailto:streetworks@york.gov.uk)
- Vehicle crossing (Section 184) - [streetworks@york.gov.uk](mailto:streetworks@york.gov.uk)
- Temporary highway closure (Road Traffic Regulation Act 1984, Section 14) [highway.regulation@york.gov.uk](mailto:highway.regulation@york.gov.uk)
- Footpath/bridleway diversion (Town and Country Planning Act 1990, Section 257)

## 3. CONTACT UTILITIES:-

You are advised that this proposal may have an effect on Statutory Undertakers equipment. You must contact all the utilities to ascertain the location of the equipment and any requirements they might have prior to works commencing.

### Contact details:

**Case Officer:** Erik Matthews  
**Tel No:** 01904 551416

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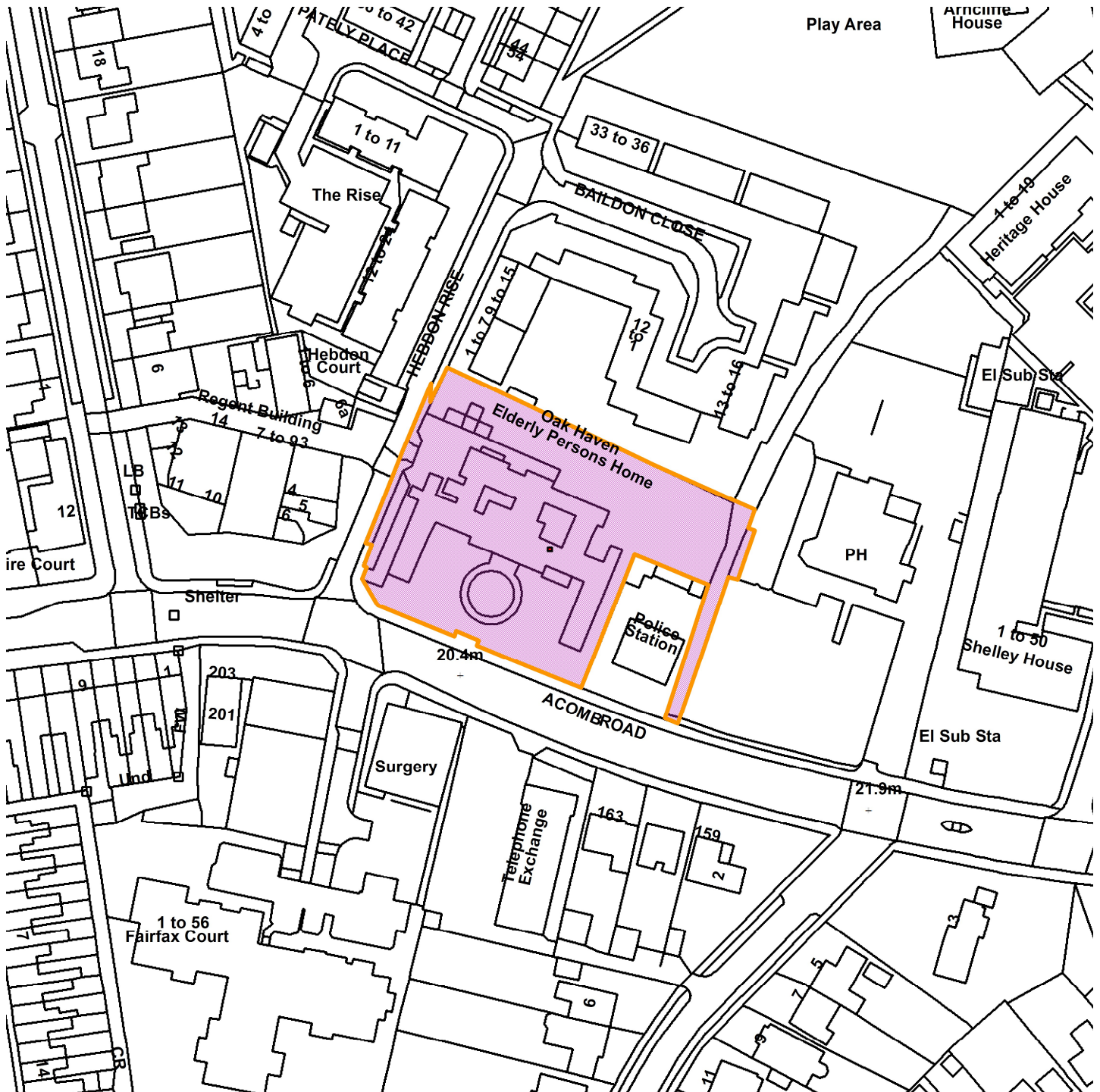


# Oak Haven, 144 Acomb Road, York YO24 4HA

22/00304/FULM



GIS by ESRI (UK)



Scale : 1:1293

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<b>Organisation</b>	City of York Council
<b>Department</b>	Directorate of Place
<b>Comments</b>	Site Location Plan
<b>Date</b>	21 July 2022
<b>SLA Number</b>	

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# Planning Committee A

To be held on Thursday 4<sup>th</sup> August 2022 at 4:30pm

# 22/00304/FULM - Oak Haven, 144 Acomb Road, York

Erection of 64 bedroom residential care home (use class C2) with associated structures, access, parking and landscaping following demolition of existing structures

Page 176

# Existing Building



# Proposed Site Plan



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- Drawing to be read in conjunction with drawing No. 3062 - JOLAN 1018 Landscape Plan. Refer to Landscape Designer's Specification.
- Site boundary
  - Building / structure to be demolished
  - Existing tree
  - Proposed tree planting  
Refer to Landscape Designer's specification
  - Proposed grassed area  
Refer to Landscape Designer's specification
  - Proposed tarmac area  
Refer to Landscape Designer's specification
  - Proposed block paving  
Refer to Landscape Designer's specification
  - Proposed paving  
Refer to Landscape Designer's specification
  - Proposed Balms  
Refer to Landscape Designer's specification
  - Retaining Wall
  - Existing Level
  - Proposed Level
  - Electric Vehicle Charging Point

REV	AMENDMENTS	ORGN	DATE
P1	Final issue	HS	09/20
P2	Revised to reflect change in missing	HS	01/20
P3	Revised to show new entrance location	HS	03/20
P4	Redline revised	HS	05/20
P5	Scheme position updated	SH	07/08/20
P6	Revised scheme to suit Planning Officers' comments	JD	20/02/20
P7	Crash Mats Extended to Parking Bay 2	HS	28/02/20
P8	Overhang columns included	PC	03/02/20
P9	Planning issue	PC	30/02/20
P10	Revised to suit Highways Comments	MS	07/04/20
P11	Landscaping updated to suit Officers comments	HS	09/08/20



ite Plan as Proposed  
1 : 200

City of York Council Planning Committee A Meeting - 4th August 2022

**PLANNING**  
**wrdunn.**  
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CLIENT  
  
**BURLINGTON CARE**  
 Caring for your family

PROJECT  
 Oakhaven Care Home  
 144 Acomb Rd,  
 Holgate,  
 York,  
 YO24 4JA

DRAWING TITLE  
 Site Plan as Proposed

SCALE @ A:	DATE	DRAWN	CHECKED
As Indicated	18.09.2020	HS	MS

DRAWING NUMBER	VERSION	AUTHORITY
4463-WRD-XX-ZZ-DR-A-0500	P11	S3

# Proposed Elevations



1 North Elevation  
1:100



5 Courtyard Elevation  
1:100



2 South Elevation  
1:100



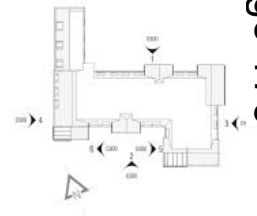
6 Courtyard Elevation  
1:100



3 East Elevation  
1:100



West Elevation  
1:100







# Visualisation 1



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# Visualisation 2



Visualisation 3



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**COMMITTEE REPORT**

**Date:** 4 August 2022      **Ward:** Rawcliffe And Clifton  
Without

**Team:** West Area      **Parish:** Rawcliffe Parish Council

**Reference:** 22/00707/FULM

**Application at:** York Wheelchair Centre Bluebeck House Bluebeck Drive York  
YO30 5RA

**For:** Erection of 72no. bedroom care home (use class C2) with  
associated landscaping following demolition of Blue Beck House  
and outbuildings

**By:** Torsion Care (York) Limited And NHS Property Services Ltd

**Application Type:** Major Full Application

**Target Date:** 21 July 2022

**Recommendation:** Approve

**1.0 PROPOSAL**

1.1 The proposal is to demolish the existing building Bluebeck House and associated buildings and construct a new 72 bed care home. Parking will be provided for the new building as well as existing parking retained for Clifton House (to the west of the site). Access to Clifton House is rerouted so that it follows the most direct route along the south of the site rather than the current route around the north of the buildings.

1.2 The new building is predominantly 3 storeys in height with 2 storey elements to the north and west ends of the building. The building is sited further to the north of the site than the existing buildings with landscaped areas to all sides. Materials are predominantly buff brick with artificial stone detailing and an artificial slate roof. The building is traditional in design with gabled elements breaking up the bulk of the building.

1.3 The site is within the general extent of the Green Belt as defined by saved policies within the within the Yorkshire and Humber Regional Spatial Strategy and also within the Green Belt as defined within the emerging Local Plan. It sits within a "Green Wedge" as identified in Fig 3.1 "Historic Character and Setting" in the Publication Draft Local Plan 2018.

1.4 Historically the site was occupied by Clifton Hospital, which was in operation until 1994 when the main hospital was demolished. This has now been replaced by a residential development. The principal building on the application site, consisting of Bluebeck House, is believed to have been constructed to accommodate the hospital laundry. To the north of Bluebeck House is a fenced compound housing a portacabin type building. The edge of the developed area is defined by a band of trees covered by the Tree Preservation Order which covers all trees within the wider hospital site.

1.5 The majority of the site is within Flood Zone 2. Areas to the north beyond the site boundary are within Flood Zone 3. To the west of the site is an area of car parking and beyond this is Clifton House, a secure hospital unit, and to the east are further modern hospital buildings. South of the site much of the former Clifton Hospital site has been redeveloped with housing although there are a couple of instances of retained and converted hospital buildings.

## **2.0 POLICY CONTEXT**

### **NATIONAL PLANNING POLICY FRAMEWORK**

2.1 The revised National Planning Policy Framework (NPPF) 2021 sets out the government's planning policies for England and how these are expected to be applied. It is a material consideration in the determination of this planning application.

2.2 Planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise (section 38(6) Planning and Compulsory Purchase Act 2004).

2.3 The Statutory Development Plan for the City of York comprises the saved policies and key diagram of the otherwise revoked Yorkshire and Humber Plan Regional Spatial Strategy (2008) and any made Neighbourhood Plan.

2.4 Although the RSS has otherwise been revoked, its policies which relate to the York Green Belt have been saved together with the Key Diagram insofar as it illustrates the general extent of the Green Belt around York. Saved policy YH9 states 'the detailed inner boundaries of the Green Belt around York should be defined in order to establish long term development limits that safeguard the special character and setting of the historic city. The boundaries must take account of levels of growth set out in the RSS and must also endure beyond the Plan period.'

2.5 The application site falls within the general extent of the Green Belt as shown on the Key Diagram of the saved RSS Green Belt policies.

#### PUBLICATION DRAFT LOCAL PLAN ("draft Local Plan")

2.6 The draft Local Plan was submitted for examination on 25th May 2018. Phase 1 of the hearings into the examination of the draft Local Plan took place in December 2019 and consultation on proposed modifications to the plan were consulted on in line with Regulation 19 in 2019 and 2021. Phases 2 of the hearings took place in earlier in 2022, phase 3 hearings are currently taking place, and phase 4 scheduled later in the year. In accordance with paragraph 48 of the NPPF the draft Local Plan policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012. (N.B: Under transitional arrangements plans submitted for examination before 24 January 2019 will be assessed against the 2012 NPPF).

2.7 Key relevant draft Local Plan policies are:

DP2 – Sustainable Development

DP3 – Sustainable Communities

SS1 – Delivering Sustainable Growth for York

SS2 – The Role of York's Green Belt

H9 – Older Persons Specialist Housing

D1 – Place Making

D2 – Landscape and Setting

D6 – Archaeology

D7 – The significance of non-designated heritage assets

GI2 - Biodiversity and access to nature

GB1 – Development in the Green Belt

CC1 – Renewable and Low Carbon Energy Generation and Storage

CC2 – Sustainable Design and Construction of New Development

ENV3 – Land Contamination

ENV5 – Sustainable Drainage

WM1- Sustainable waste management

T1 – Sustainable Access

T7 – Minimising and Accommodating Generated Trips

## 2.8 Emerging Local Plan evidence base

The evidence base that underpins the proposed emerging policies is considered to be a material consideration in the determination of this planning application. The directly relevant evidence base is:

- Topic Paper 1: Approach to defining York's Green Belt (2021).
- Approach to the Green Belt Appraisal and Maps (2003).

## DRAFT LOCAL PLAN 2005

2.9 The City of York Draft Local Plan incorporating the Fourth Set of Changes Development Control Local Plan (April 2005) was approved for Development Management purposes. The 2005 plan does not form part of the statutory development plan for the purposes of S38 (6) of the Planning and Compulsory Purchase Act 2004. Its policies are however considered capable of being material considerations in the determination of planning application where policies relevant to the application are consistent with those in the NPPF although the weight that can be attached to them is very limited.

## 3.0 CONSULTATIONS

### 3.1 Public protection

Conditions recommended regarding submission of a CEMP, construction working hours, EVCP, land contamination, noise, odour and lighting controls.

### 3.2 Design, conservation and sustainable development (Archaeology)

A desk based assessment has been submitted. The site contains one of the few surviving late 19th century Clifton hospital buildings (Bluebeck House) designed by George Fowler Jones. This can be described as a non-designated heritage asset. The building has been subject to alteration although the upper floors less so. As the building is now proposed for demolition a photographic recording of the structure should be undertaken. As advised by the conservation officer this should be to a



Level 3 recording given that the building will be demolished. This can be secured by condition. In terms of below-ground archaeological impact the site has low potential to contain significant archaeology. No intrusive investigation or monitoring will be required.

### 3.3 Design, conservation and sustainable development (Ecologist)

No objections subject to conditions regarding a requirement for a European Protected Species Licence in relation to bats; to secure biodiversity net gain; a CEMP for biodiversity; lighting plans; and protection of nesting birds.

### 3.4 Design, conservation and sustainable development (Landscape Architect)

Existing trees play a considerable part in the amenity of the site and surrounding landscape. Further information is requested to indicate that development can take place without harm to the trees. Concern is expressed about the proximity of the building at the northern end to the trees and the potential for overshadowing in the summer.

### 3.5 Design, conservation and sustainable development (Conservation)

The the building was constructed to accommodate the laundry to the Clifton Hospital in the late 19<sup>th</sup> century. The design of the building is of high quality and relates to its historic function and association with the hospital. Few alterations have been made except for a modest rear extension. It is considered that the building has architectural and heritage interest and qualifies as a non-designated heritage asset defined in the Planning Practice Guidance as “having a degree of significance meriting consideration in planning decisions”. Para. 203 of the NPPF is relevant and requires a balanced judgement having regard to the scale of any harm or loss and the significance of the heritage asset.

### 3.6 Carbon reduction team

The design document provided with the applications makes reference to a future sustainability strategy which will establish levels of efficiency and low energy use. The future design will consider photovoltaics. As the application progresses we would expect to see this sustainability strategy, with a full breakdown of predicted

energy use (BER, TER, and DER), low carbon technologies which will be used, and the carbon savings the efficiency measures will achieve. Conditions recommended.

### 3.7 Flood risk management team

Foul water is to connect to the public foul water sewer serving Clifton House and the nearby residential development. This has been agreed with Yorkshire Water. In relation to the surface water strategy no evidence has been provided of the existing impermeable areas or evidence of the suitability of the proposed outfall.

### 3.8 Rawcliffe Parish Council

No objections but wish to ensure that there are sufficient parking spaces for staff and visitors.

### 3.9 Yorkshire Water

No objections, conditions recommended.

### Highways Network Management

3.10 Issues were raised around the proximity of the visitor cycle parking to the entrance, details of the travel plan and access through to Clifton House. Conditions recommended.

## **4.0 REPRESENTATIONS**

### Neighbour notification and publicity

4.1 Three letters of objection from residents raising the following issues:

- Express concerned about loss of heritage
- The building is included in the Local List
- Aesthetic value of building
- Environmental impact of demolition compared with re-use
- Impact on wildlife on nearby Rawcliffe Meadows SSSI
- Wildlife reports with application are inadequate and undertake at wrong time of year

4.2 A letter of objection was received from the NHS at neighbouring Clifton House. This raised concern around access during construction, access to parking spaces and a compound at the north of the site. Following discussion with the developer this objection has now been withdrawn.

4.3 One letter of support querying the future of Verandah Cottages. Officers note that these properties are outside the red line boundary and do not form part of the current proposal.

## **5.0 APPRAISAL**

### Key Issues

5.1 The key issues are as follows:

- Principle of development
- Need for older persons accommodation
- Heritage impact
- Design and layout of the site
- Trees and landscape
- Highways and access
- Drainage and flood risk
- Residential amenity and public protection
- Ecology
- Sustainability
- Very special circumstances and the planning balance.

### PRINCIPLE OF DEVELOPMENT

5.2 For the purposes of s.38(6) Planning and Compulsory Purchase Act, the proposals should be assessed against the saved RSS Green Belt policies. Policies contained within the National Planning Policy Framework are also material considerations.

5.3 The 2005 DCLP shows the site as washed over by the Green Belt. In contrast the emerging Local Plan (proposed modifications 2021) shows the Clifton Hospital as excluded from the Green Belt. Details of this are included within Topic Paper 1 Green Belt Addendum January 2021, Annex 3 Inner Boundary EX/CYC/59c, and Annex 6 Proposed modifications EX/CYC/59h. These documents note that the Clifton hospital site is completely developed with no sense of openness and no relationship to the wider countryside. For this reason, it serves no Green Belt

purpose and it has been proposed that the Green Belt boundary follows the edge of the built development.

5.4 It is the Local Planning Authority's position that until a Local Plan for the City of York is adopted, development management decisions relating to proposals falling within the general extent of the Green Belt are made on the basis that the land should be treated as Green Belt. Therefore, Green Belt policies set out within the NPPF apply to the determination of this development proposal.

5.5 Paragraph 147 of the NPPF states: 'Inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances'. Paragraph 148 goes on to state: 'When considering any planning application, local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. 'Very special circumstances' will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations'.

5.6 Paragraph 149 of the NPPF states: 'A local planning authority should regard the construction of new buildings as inappropriate in the Green Belt.' There are a number of exceptions to this statement, however the proposal does not fall within any of these, and it is therefore considered to represent inappropriate development.

## IMPACT ON THE OPENNESS OF THE GREEN BELT

5.7 As set out in Paragraph 137 of the NPPF. One of the essential characteristics of Green Belts are their openness and their permanence. There is no definition of 'openness' in the NPPF. However, it is commonly taken to mean the state of being free from development, the absence of buildings and relates to the quantum and extent of development and its physical effect on the site.

5.8 Policy GB1 of the 2018 Draft Plan states that permission will only be granted for development where:

- The scale, location and design of development would not detract from the openness of the Green Belt;
- It would not conflict with the purposes of including land within the Green Belt; and
- It would not prejudice or harm those elements which contribute to the special character and setting of York.

5.9 There are unresolved objections to Policy GB1 that will be considered through the examination in public of the draft Local Plan and therefore it should only be afforded limited weight in the decision making process for the purposes of this application.

5.10 The proposal removes the existing building and replaces it with a new building. The Planning Statement submitted with the application notes that the volume of the existing structures to be demolished is 7483m<sup>3</sup> and the replacement building is 16150m<sup>3</sup>. The proposed site plan shows the existing building footprints in comparison to the proposed. At present, the site is almost entirely hard standing outside of the building footprints; the proposal results in a 128% increase in soft landscaping on site. The proposed building clearly represents an increase in size and scale of the buildings which in turn would therefore have an impact on openness. The existing developed nature of the site along with the significant increase in soft landscaping of the site would also contribute towards mitigation which is discussed in the conclusion.

## IMPACT ON GREEN BELT PURPOSES

5.11 Paragraph 138 of the NPPF sets out that the Green Belt serves five purposes. These are:

- To check the unrestricted sprawl of large built up areas;
- To prevent neighbouring towns merging into one another;
- To assist in safeguarding the countryside from encroachment;
- To preserve the setting and special character of historic towns; and
- To assist in urban regeneration, by encouraging the recycling of derelict land and other urban land.

5.12 The primary purpose of the York Green Belt is to safeguard the special character and setting of the historic city as referred to in Policy YH9C of the RSS and Policy SS2 of the 2018 draft Local Plan, although weight can only be attached to the latter. Topic Paper 1: Approach to defining York's Green Belt Addendum (2021) identifies that the site does not serve Green Belt purposes. The site lies within an area which is completely developed and has no relationship with the wider countryside. As such it is accepted that the application site, which forms part of the wider developed Clifton Park Hospital area, does not contribute towards the purposes of including land within the Green Belt. The modifications to the emerging

Local Plan included in Topic Paper 1 (2021) recognise this and remove the site from the Green Belt. The proposed new Green Belt boundary will follow the boundary of the existing built area.

## NEED FOR OLDER PERSONS ACCOMMODATION

5.13 The current national and local focus for older persons accommodation is to try to meet care and support needs at home to enable independence. As such the Council's preference would be for independent living or independent living with care accommodation on the site. The developer has noted that there has been a significant decline in the supply of care bed spaces in the UK in recent years. They have commissioned a report on care homes in York and the surrounding catchment. This found that by 2023 an additional 595 care home beds will be needed in the York area. Currently, there are a total of 1281 care beds in the area within 25 separate care home facilities. Out of these 25 care homes, only 8 have been built since 2000, leaving many of those built prior to 2000 in converted, non-purpose-built facilities. A number of these care homes do not have en-suite bathroom facilities which has been a requirement for all new build care homes since April 2002. The proposal is therefore to provide a care home to help address these needs.

5.14 Policy H9 of the draft Local Plan addresses the issue of older persons specialist housing. It notes that such development will be supported where it meets an identified need; is well-designed to meet the particular requirements of residents; and is in an accessible location by public transport, or within walking distance to a range of community facilities.

## HERITAGE IMPACT

5.15 Bluebeck House is considered to represent a non-designated heritage asset. It is one of few remaining buildings on the site of the now-demolished Clifton Hospital. The wider, former hospital site is of importance due to it having been chosen for the new asylum as an airy, accessible site between Clifton Ings and Shipton Road, providing an extensive parkland setting. While the site has been lost due to extensive demolitions and redevelopment, the landscape of the site remains legible and distinctive in character; the extant buildings, including Bluebeck House, punctuate the landscape orientating and referencing the history of the site. Thought to have been the hospital's laundry, Bluebeck House was completed in 1891-7. The design and construction of the building is of high quality and possesses value as a result of this; also, historical value relating to its original function and association

with the hospital; and communal heritage value relating to the historic and current uses for health services.

5.16 The building is identified on the draft Local List held by York Civic Trust. This List cannot be given any statutory weight as it has not been through any approval process with the Council. This does not alter the consideration of the building as a non-designated heritage asset.

5.17 Policy D7 of the draft Local Plan refers to non-designated heritage assets, and notes that development proposals will be encourage and supported where they are designed to sustain and enhance the significance of York's historic environment, including non-designated heritage assets. The significance of non-designated heritage assets and their setting should be assessed and development which would remove, harm or undermine the significance of such assets will only be permitted where the benefits of the development outweigh the harm having regard to the scale of the harm and significance of the heritage asset. The policy goes on to require that, prior to any works being undertaken, appropriate building recording is completed.

5.18 Para.203 of the NPPF requires that the effect of an application on the significance of a non-designated heritage asset should be taken into account in determining an application. A balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset.

5.19 A heritage assessment has been submitted with the application and this assesses the significance of Bluebeck House both as a historic building and in relation to its wider context. This report notes that there are no designated heritage assets on site and the proposal would not impact on the setting of any designated heritage assets. It goes on to identify that Bluebeck House did not form part of the original Clifton Hospital complex however much of its setting has been radically impacted by the demolition and redevelopment of the rest of the site. It suggests that it is no longer possible to understand the building in its historic context, therefore setting makes no contribution to the significance of the building. The interest of the building is considered to be in relation to its original purpose to accommodate inmates in their working environment.

5.20 Consideration has been made of the possibility of conversion of the building. The developer notes that conversion would result in a maximum of 26 bed spaces which would not prove viable. Care Quality Commission standards in relation to

windows and access would not be achievable. It is also noted that the building has a very deep floor plan making it difficult to get natural light to the centre of the structure. Finally, to achieve the energy efficiency requirements required by the developer in a converted building, and additionally maintain a historic building, would not prove viable.

5.21 Assessment has been made of the significance of Bluebeck House and has identified that it was a later addition to the Clifton Hospital complex, although designed by a local architect who was involved with the site over a long period in the second half of the nineteenth century. The building accommodated female inmates who worked within the laundry building to the rear. The loss of much of the rest of the hospital buildings has eroded the setting of Bluebeck House and reduced its significance however it is still of local interest and emerging plan policy D7 and NPPF para.203 are relevant, and a balanced judgement required.

## DESIGN AND LAYOUT OF THE SITE

5.22 The building has been designed to respond to its context. While predominantly 3 storey in height, it reduces to the north addressing the undeveloped area beyond the site. The eastern wing of the building remains at 3 storeys mimicking the scale of the existing Bluebeck House and providing a focal point for Bluebeck Drive. The western wing again reduces in scale to reflect the scale of Clifton house.

5.23 The building features pitched roofs and gables taking reference from Bluebeck House and other retained historic buildings on the wider site. Materials are predominantly buff brick with art stone detailing and artificial slate roofs and the design is a contemporary interpretation of the traditional building forms still remaining from the Clifton Hospital site.

5.24 There is little context for the building as buildings to both sides are contemporary and functional in their appearance. Housing to the south of the site is typical of a modern development and is visually separate from the site. To this extent, the building is considered acceptable in terms of policy D1 of the draft Local Plan which requires that development makes a positive design contribution of the city. Policy D1 also requires that density is appropriate for the proposed use and context; that the development does not dominate other buildings and spaces; proposals should not be a pale imitation of past architectural styles and appropriate materials should be used; create buildings that are fit for purpose but also adaptable



to respond to change; and create places that feel true to their intended purpose. It is considered that the proposal meets these requirements.

5.25 The large footprint of the building is noted however it is appropriate in this location where historically there have been large buildings, and more recent structures are similarly less domestic in scale. As stated above, the site remains quite visually, and functionally, separate from the residential development to the south. The proposed care use also fits well here with other health related uses and reflecting the historic use of the site.

## TREES AND LANDSCAPE

5.26 Policy D2 of the draft Local Plan refers to impacts on landscape and setting. It requires that development conserves and enhances landscape quality and character. The significance of landscape features, such as mature trees, should be recognised and retained in a respectful context and landscape schemes should be sustainable, practical and of a high quality. Relationships between the built and natural environment should be comfortable and appropriate in scale.

5.27 Trees to the north and east of the site are large and dominant, and protected by a group TPO, although outside the boundaries of the development. These trees play a considerable part in the amenity of the site and surrounding landscape, and will, following adoption of the Local Plan, form the boundary to the Green Belt.

5.28 The site plan indicates the root protection zone of the retained trees is away from the proposed building however there may be some pressures during construction at the northern end of the building. The applicant has confirmed that construction can be undertaken outside of tree protection fencing and that trees can be adequately protected during construction.

5.29 The other issue related to trees in proximity to buildings related to amenity and issues with overshadowing and loss of light. The trees sit to the north of the building and as such will not directly overshadow it. The building has been designed to avoid conflict and pressure to prune trees as a result of any overbearing impact on rooms from the proximity and scale of the trees. While the structure itself is approximately 5m at its closest point to the tree canopies, bedroom windows do not look to the north. Windows in the northern elevation serve communal areas on the ground and first floor with a balconied communal area on the second floor. These communal

areas have windows to three elevations allowing aspects on to the garden areas as well as towards the trees.

5.30 It is probably in relation to the usability of the outside space that the proximity of the trees will have most impact. Again, the external space to the north will not be directly overshadowed, plans show that the trees are set back from the boundary and the canopy only slightly overhangs the site. However, the space to the north of the building is relatively narrow and the scale of the trees and building will make this a less appealing area. To compensate for this, there are a number of other more attractive external spaces around the buildings. The large communal room to the west of the building opens on to an external seating area which faces west and would be an attractive area for residents. Similarly, the external space to the north, while close to the trees, is open to the east while also enjoying the leafy backdrop provided by the trees. There is a range of different external areas around the building that allows residents a choice of seating and which seems appropriate for their needs.

## HIGHWAYS AND ACCESS

5.31 In terms of highways, the site is currently served by an access road which leads round the north of the site and provides access to Clifton House. The proposal is to amend this access to follow the southern boundary of the site retaining access to Clifton House at all times. Existing parking for that site is to be retained and is indicated on the site plan. Parking for the new care home will be sited to the east of the site adjacent to the boundary with Clifton Park Treatment Centre.

5.32 Cycle parking for staff will be provided to the west of the building, adjacent to the staff entrance, and is in accordance with guidelines. Visitors cycle parking is on the eastern side of the building near to the main entrance. A travel plan has been submitted with the application which seeks to promote sustainable means of transport.

## DRAINAGE AND FLOOD RISK

5.33 The site is within Flood Zone 2. A sequential test has been undertaken by the Applicant, in accordance with Section 14 of the NPPF. This looks at any sites within the local authority area which can accommodate the proposed site, and at a lower flood risk than the application site. For a site to be considered available, it must offer a realistic prospect of development within a reasonable period of time and must not

be allocated for an alternative use, benefitting from an existing planning permission, or already under construction. Thirty potential sites were identified, of which 26 were either too small or too large (ie more than 100% larger than the application site). Of the remaining 4 sites, 2 already have planning permission and are therefore not considered available. The remaining sites (allocated as H39: North of Church Lane, Elvington and H58: Clifton without Primary School) were also discounted. H39 is approximately 9.5km from the city centre and situated within a village. Information in the CYC Strategic Housing Land Availability Assessment notes objection to the site in relation to access and lack of capacity in local facilities. The site was discounted because of its distance to the city and village location. H58: Clifton without Primary School is identified in an Executive report by the Council in January 2022 for redevelopment with a library and affordable housing. The parcel of land to deliver affordable housing is 0.25Ha and therefore too small for the proposed development. No sequentially preferable sites were identified.

5.34 A Flood Risk Assessment has been submitted with the application and has assessed that the risk of future flooding is low and can be mitigated by the use of raised finish flood levels above the 1 in 1000 year level. There is no requirement to complete the exception test given that the site is in Flood Zone 2 and the use falls within the 'More vulnerable' category.

5.35 Details of drainage can be covered by planning conditions.

## RESIDENTIAL AMENITY AND PUBLIC PROTECTION

5.36 Policy D1 of the draft Local Plan addresses issues of amenity and requires that new development consider residential amenity so that residents living nearby are not unduly affected by noise, disturbance, overlooking or overshadowing. The proposed building is well sited to ensure minimal impacts on neighbouring residents. The building is approximately 20m from the boundary of the residents on Fylingdales Avenue, south of the site, and 30m from the back of these houses. Although the new building will be 3 storey and will have bedroom windows looking towards the existing properties, the distances are acceptable to prevent overlooking. It is also noted that an existing substantial, historic boundary wall will be retained along this edge of the site, separating the residential development from the new one and the existing building has windows facing in this direction and at a similar distance.

5.37 Two period cottages, Verandah Cottages, to the southeast of the site will be in closer proximity to the new building. These properties are currently unoccupied and have previously been occupied by the NHS having been constructed as dwellings in conjunction with Clifton Hospital. These cottages do not face on to the site. The side of the closest cottage is approximately 10m from the corner of the new building which will look on to the rear garden of these cottages. This relationship is quite tight but little different to the existing one, and there is no overlooking directly into the properties.

5.38 The nearest part of the new building to Clifton House drops down to two storeys and does not contain windows in the end gable facing west. Other west facing windows in the main building are approximately 25m from the boundary with Clifton House and not considered to result in any overlooking.

5.39 The scale and siting of the building, particularly when taking into consideration that there is currently a building on site, is considered to ensure that there is no overbearing or overshadowing impact on neighbouring residents. The proposed care home use is not considered to result in any significant increase in noise that would impact on neighbouring residents. There may be some small increase in noise to the rear of properties on Fylingdales Avenue as a result of the re-routing of the access to the back of their properties as opposed to the current situation where it goes to the north of Bluebeck House. However, there is a significant boundary wall to be retained which will provide good separation and the level of traffic is considered low given that the road only provides access to Clifton House.

## ECOLOGY

5.40 Bluebeck House contains a pipistrelle bat roost. As the proposed work would result in the permanent loss of the roosts in Bluebeck House, a European Protected Species licence from Natural England will be required prior to the commencement of works.

5.41 The species protection provisions of the Habitats Directive, as implemented by the Conservation of Habitats and Species (Amendment) (EU Exit) Regulations 2019, contains three "derogation tests" which must be applied by Natural England when deciding whether to grant a licence to a person carrying out an activity which would harm a European Protected Species (EPS). Notwithstanding the licensing regime, the LPA must also address its mind to these three tests when deciding whether to grant planning permission for a development which could harm a EPS.

5.42 The “derogation tests” which must be applied for an activity which would harm a European Protected Species (EPS) are contained within the species protection provisions of the Habitats Directive, as implemented by the Conservation of Habitats and Species Regulations 2017 (as amended) are as follows:

- 1) that the action is for the purpose of preserving public health or public safety or other imperative reasons of overriding public interest including those of a social or economic nature;
- 2) that there is no satisfactory alternative; and
- 3) that the action authorised will not be detrimental to the maintenance of the species concerned at a favourable conservation status in their natural range.

5.43 With regards to the third test, the conservation status of species will be taken as 'favourable' when:

- a) population dynamics data on the species concerned indicate that it is maintaining itself on a long-term basis as a viable component of its natural habitats, and
- b) the natural range of the species is neither being reduced for the foreseeable future, and
- c) there is, and will probably continue to be, a sufficiently large habitat to maintain its populations on a long-term basis.

5.44 With regards to test 3, the Bluebeck House currently supports a common pipistrelle maternity roost and a common pipistrelle day roost. Common pipistrelle bats are widespread throughout the UK and classed as a species of 'least' conservation concern. The requirement for a European Protected Species Licence will prevent any direct harm and the provision of new roost sites will maintain roosting opportunities on site. Therefore, the third test for maintenance of favourable conservation status is met. A condition is suggested to cover these requirements.

5.45 The development is required to provide biodiversity enhancement in accordance with para.174d of the NPPF. The Ecological Impact Assessment provides Biodiversity Net Gain Metric 3.0 calculations that show the development will be beneficial to local ecology through the provision of green spaces within the application site. To ensure that these enhancements are implemented a condition is suggested.

5.46 Policy GI2 of the draft Local Plan requires that development close to local wildlife sites does not result in any harm to these sites. Clifton Ings and Rawcliffe Meadows Site of Special Scientific Interest (SINC) and Rawcliffe Meadows Site of Interest for Nature Conservation (SINC) are all within close proximity to the site and therefore, to ensure that these areas are safeguarded during the construction phase of development a condition for a CEMP (Biodiversity) is recommended.

## SUSTAINABILITY

5.47 Para.120 of the NPPF requires that planning decisions give substantial weight to the value of using suitable brownfield land within settlements for homes and other identified needs. The proposal meets this requirement providing specialist older persons accommodation.

5.48 In terms of sustainable design and construction, the scheme recognises the need to meet the requirements of Building Regulations and the Council's policy CC1 and CC2. The design includes photovoltaics on the south facing roof slopes and mentions consideration of air source heat pumps and mechanical heat recovery. Conditions are recommended to ensure compliance with Local Plan policy.

5.49 The site is recognised as being somewhat separate from local services. However, the developer notes that the care home would operate as a secure unit which does not allow residents to freely enter and exit without supervision. To this effect, the proximity of services is less relevant to future residents and the site is more suitable for this type of care than other more independent living. The site is also recognised to be well connected by public transport and cycle routes to the city centre and as such is relatively sustainable for staff and visitors.

## THE PLANNING BALANCE AND VERY SPECIAL CIRCUMSTANCES

5.50 The proposed development represents inappropriate development in the Green Belt. Paragraph 147 of the NPPF explains that inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances. Paragraph 148 says when considering any planning application, local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. 'Very special circumstances' will not exist unless the potential harm to the Green Belt by reason of inappropriateness and any other harm resulting from the proposal, is clearly outweighed by other considerations.

5.51 As well as the harm resulting from inappropriateness, other harm has been identified as a loss of openness. It is also noted that the loss of the non-designated heritage asset, Bluebeck House weighs against the scheme.

5.52 The following considerations have been put forward as very special circumstances:

- The intention of the draft Local Plan is to remove the site from the Green Belt as it is completely developed and has no sense of openness or relationship with the Green Belt;
- The site is brownfield;
- A need for more older persons accommodation within York – the developers' assessment identifies a shortfall of 595 care room beds, and notes that a number of existing facilities in the city do not meet modern standards;
- The provision of care bed spaces will free up general housing stock;
- Creation of 70 new jobs (50 full time, 20 part time).

These very special circumstances are considered to outweigh the harm through inappropriateness, and other identified harm, as required by paragraph 147 of the NPPF.

5.53 In accordance with paragraph 203 of the NPPF, a balanced judgement is required when considering harm to a non-designated heritage asset. In this instance, the significance of the asset is already considered reduced, as a result of the loss of much of the original Clifton hospital site through redevelopment, impacting on the setting of Bluebeck House. The scheme proposed results in benefits including the provision of 72 care bed spaces in a purpose built modern building addressing an identified need for older persons accommodation; the creation of 70 jobs; and biodiversity net gain of 137% habitat units resulting from the increase in soft landscaping on site of approximately 120%. In addition, the issues around conversion of the building are noted, in particular the deep footprint of the building and difficulties around bringing the building up to current energy efficiency standards are recognised. Paragraph 120 of the NPPF gives substantial weight to the use of suitable brownfield land within settlements for homes and other identified needs, and it is considered that this also adds weight in favour of the scheme. These benefits of the scheme are considered sufficient to outweigh the loss of the non-designated heritage asset, in accordance with paragraph 203.

## 6.0 CONCLUSION

6.1 Detailed issues related to the design and form of the building, access, landscaping, and biodiversity have been resolved and are considered to comply with relevant policies. In addition, issues of neighbouring residential amenity are considered to be addressed in the design and siting of the building in relation to existing properties. The proposal complies fully with the requirements of policy H9 in relation to older persons accommodation in meeting an identified need, being well designed and in an accessible location by public transport.

6.2 The site falls within the general extent of the Green Belt and the scheme is considered to be inappropriate by definition. However, officers consider that the proposed very special circumstances are sufficient to outweigh the harm to the Green Belt through inappropriateness and other identified harm, including the loss of the non-designated heritage asset, in accordance with para.148 of the NPPF. It is particularly noted that there is a significant need for older persons accommodation in the city. In addition the site is identified, within the evidence base documents for the draft Local Plan, as not serving any Green Belt purpose and is therefore intended to be removed from the Green Belt following adoption of the Local Plan.

6.2 The proposal is considered to comply with relevant policies of the draft Local Plan and represents an appropriate re-use of this brown field site.

## 7.0 RECOMMENDATION: Approve

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans and other submitted details:-

Location plan

Proposed elevations 3165-HIA-01-XX-DR-A-0301 P04, 3165-HIA-01-XX-DR-A-0302 P3 and 3165-HIA-01-XX-DR-A-0303 P3

Proposed floor plans 3165-HIA-01-00-DR-A-0201 P5, 3165-HIA-01-01-DR-A-0211 P5 and 3165-HIA-01-02-DR-A-0221 P5

Proposed roof plan 3165-HIA-01-03-DR-A-2701 P03

Proposed site plan 3165-HIA-ZZ-XX-DR-A-0103-P8

Proposed landscape plan R3-509-03-LA-01C

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 Prior to commencement of the development, a Construction Environmental Management Plan (CEMP) for minimising the creation of noise, vibration and dust



during the demolition, site preparation and construction phases of the development shall be submitted to and approved in writing by the Local Planning Authority. The CEMP must include a site specific risk assessment of dust impacts in line with the guidance provided by IAQM (see <http://iaqm.co.uk/guidance/>) and include a package of mitigation measures commensurate with the risk identified in the assessment. All works on site shall be undertaken in accordance with the approved scheme, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To protect the amenity of the locality

4 All demolition and construction works and ancillary operations, including deliveries to and dispatch from the site shall be confined to the following hours:

Monday to Friday 0800 to 1800 hours

Saturday 0900 to 1300 hours

Not at all on Sundays and Bank Holidays.

Reason: To protect the amenity of local residents.

5 There shall be adequate facilities for the treatment and extraction of cooking odours. Details of any extraction plant or machinery and any filtration system required shall be submitted to the local planning authority for written approval prior to installation. The facilities shall be installed and fully operational as approved before the proposed use of cooking facilities commences and shall be appropriately maintained and serviced thereafter in accordance with manufacturer guidelines.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

6 Before the occupation of the development 2 Electric Vehicle Recharging Point(s) shall be provided in a position and to a specification to be first agreed in writing by the local planning authority (active provision). In addition, a minimum of 2 additional parking bays should be identified for the future installation of additional Electric Vehicle Charging Points. Such additional bays should be provided with all necessary ducting, cabling and groundwork to facilitate the addition of Electric Vehicle Charge Points in the future, if required (passive provision). The locations of these additional bays should be approved in writing by the local planning authority. Charging points should be located in a prominent position on the site and should be marked for the exclusive use of zero emission vehicles.

Reason: To promote and facilitate the uptake of electric vehicles on the site in line with the Council's Low Emission Strategy (LES) and the National Planning Policy Framework (NPPF).

7 The demolition of Bluebeck House shall not commence unless the local planning authority has been provided with either:

a) a licence issued by Natural England pursuant to Regulation 53 of The Conservation of Habitats and Species Regulations 2017 (as amended) authorising the specified activity/development to go ahead; or

b) Confirmation that the site is registered on a Bat Mitigation Class licence (formally Low Impact Class Licence) issued by Natural England; or

c) a statement in writing from the relevant licensing body to the effect that it does not consider that the specified activity/development will require a licence.

Reason: To ensure bats are protected from harm during demolition works. All British bat species and their roosts are protected by the Wildlife and Countryside Act 1981 (as amended) and the Conservation of Habitats and Species Regulations 2017 (as amended).

8 A biodiversity enhancement plan/drawing shall be submitted to, and be approved in writing by, the local planning authority prior to the commencement of works. The plan should include, but not be limited to, the recommendations set out in the Ecological Impact Assessment provided by Futures Ecology (March 2022).

Reason: To take account of and enhance the biodiversity and wildlife interest of the area, and to be in accordance with Paragraph 174 d) of the NPPF (2021) to contribute to and enhance the natural and local environment by minimising impacts on and providing net gains for biodiversity, including establishing coherent ecological networks that are more resilient to current and future pressures.

9 Prior to the installation of any new external, a 'lighting design plan' shall be submitted to and approved in writing by the local planning authority. The approved details shall be fully implemented prior to first operation and maintained for the lifetime of the development.

The plan shall:

- Demonstrate that required external lighting has been selected in-line with current guidance - Bat Conservation Trust (2018) Bats and artificial lighting in the UK.

- Demonstrate how and where external lighting will be installed (through the provision of appropriate lighting contour plans and technical specifications), clearly demonstrated where light spill will occur.

Reason: To ensure bats and their habitat are protected following the completion of works. All British bat species and their roosts are protected by the Wildlife and

Countryside Act 1981 (as amended) and the Conservation of Habitats and Species Regulations 2017 (as amended).

10 No development shall take place until a construction environmental management plan (CEMP: Biodiversity) has been submitted to and approved in writing by the local planning authority prior to the commencement of works. The CEMP: Biodiversity shall include the following:

- a) Risk assessment of potentially damaging construction activities
- b) Identification of 'biodiversity protection zones'
- c) Practical measures (both physical measures and sensitive working practices) to avoid or reduce impacts during construction (may be provided as a set of method statements)
- d) The location and timing of sensitive works to avoid harm to biodiversity features
- e) The times during construction when specialist ecologists need to be present on site to oversee works
- f) Responsible persons and lines of communication
- g) The roles and responsibilities on site of an ecological clerk of works (ECoW) or similarly competent person
- h) Use of protective fences, exclusion barriers and warning signs

Reason: To facilitate the protection of notable/sensitive ecological features and habitats on the application site and within the local area in line with Policy GI2 in the Publication Draft Local Plan (2018).

11 No vegetation removal or demolition work shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a detailed check of vegetation and structures for active birds' nests immediately before the works and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the local planning authority prior to any vegetation removal or demolition works commencing.

Reason: To ensure that breeding birds are protected from harm during construction. All British birds, their nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife and Countryside Act 1981, as amended.

12 The site shall be developed with separate systems of drainage for foul and

surface water on and off site.

Reason: In the interest of satisfactory and sustainable drainage.

13 Prior to the commencement of construction works details of the proposed means of foul and surface water drainage, including details of any balancing works and off site works, shall be submitted to and approved in writing by the Local Planning Authority and the development shall be carried out in accordance with the approved details.

Reason: So that the Local Planning Authority may be satisfied with these details for the proper and sustainable drainage of the site.

14 Prior to first occupation details of all machinery, plant and equipment to be installed in or located on the premises, which is audible outside of the premises, shall be submitted to the local planning authority for approval in writing. These details shall include average sound levels (LAeq), octave band noise levels and any proposed noise mitigation measures. The machinery, plant or equipment and any approved noise mitigation measures shall be fully implemented and operational before the proposed use first opens and shall be appropriately maintained thereafter.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

15 The approved landscape scheme for soft works (R3-509-03-LA-01C) shall be implemented within a period of six months of the practical completion of the development. Any trees or plants which within a period of ten years from the substantial completion of the landscape planting works die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of a similar size and species, unless the Local Planning Authority agrees alternatives in writing.

Reason: The landscape scheme is integral to the amenity of the development.

16 Before the commencement of development (including demolition, excavations, and building operations), a complete and detailed Arboricultural Method Statement and scheme of arboricultural supervision regarding protection measures for existing trees within and adjacent to the application site shown to be retained on the approved drawings, shall be submitted to and approved in writing by the Local Planning Authority. Amongst other information, this statement shall include details and locations of protective fencing, ground protection, a schedule of tree works if applicable, site rules and prohibitions, phasing of protection measures, site access during demolition/construction, types of construction machinery/vehicles to be used (including delivery and collection lorries and arrangements for loading/off-loading),

specialist construction techniques where applicable, parking arrangements for site vehicles, locations for stored materials, and means of moving materials around the site, locations and means of installing utilities, location of site compound. The content of the document shall be strictly adhered to throughout development operations. A copy of the document will be available for reference and inspection on site at all times.

Reason: To ensure every effort and reasonable duty of care is exercised during the development process in the interests of protecting the existing trees shown to be retained which are considered to make a significant contribution to the amenity and setting of the development and the surrounding landscape.

17 The development hereby approved shall achieve a water consumption rate of 110 litres per person per day (calculated as per Part G of the Building Regulations).

Reason: To fulfil the environmental objectives of the NPPF and support the transition to a low carbon future, and in accordance with policies CC1 and CC2 of the Publication Draft Local Plan 2018.

18 Prior to occupation details of the cycle parking areas, including means of enclosure, shall be submitted to and approved in writing by the Local Planning Authority. The building shall not be occupied until the cycle parking areas and means of enclosure have been provided within the site in accordance with such approved details, and these areas shall not be used for any purpose other than the parking of cycles.

Reason: To promote use of cycles thereby reducing congestion on the adjacent roads and in the interests of the amenity of neighbours.

19 The building shall not be occupied until the areas shown on the approved plans for parking and manoeuvring of vehicles (and cycles, if shown) have been constructed and laid out in accordance with the approved plans, and thereafter such areas shall be retained solely for such purposes.

Reason: In the interests of highway safety.

20 Prior to occupation details of the bin storage areas, including means of enclosure, shall be submitted to and approved in writing by the Local Planning Authority. The building shall not be occupied until the bin storage areas and means of enclosure have been provided within the site in accordance with such approved details, and these areas shall not be used for any purpose other than the storage of bins.

Reason: In the interests of visual and residential amenity.

21 Notwithstanding the supporting Travel Plan, the development hereby approved shall not be occupied until a Full Travel Plan has been submitted and approved in writing by the LPA. The Travel Plan should be developed and implemented in line with local and national guidelines and the submitted Interim Travel Plan. The site shall thereafter be occupied in accordance with the aims, measures and outcomes of approved Travel Plan.

Within 12 months of occupation of the site a first year travel survey shall have been submitted to and approved in writing by the Local Planning Authority. Results of yearly travel surveys shall then be submitted annually to the authority's travel plan officer for approval.

Reason: To ensure adequate provision is made for the movement of vehicles, pedestrians, cycles and other forms of transport to and from the site, together with parking on site for these users and to comply with paragraph 111 of the National Planning Policy Framework.

22 Notwithstanding any proposed materials specified on the approved drawings or in the application form submitted with the application, samples of the external materials to be used shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the construction of above foundation works. The development shall be carried out using the approved materials.

Note: Because of limited storage space at our offices it would be appreciated if sample materials could be made available for inspection at the site. Please make it clear in your approval of details application when the materials will be available for inspection and where they are located.

Reason: So as to achieve a visually cohesive appearance.

23 Prior to development, an investigation and risk assessment (in addition to any assessment provided with the planning application) shall be undertaken to assess the nature and extent of any land contamination. The investigation and risk assessment must be undertaken by competent persons. A written report of the findings shall be produced, submitted to and approval in writing of the Local Planning Authority. The report of the findings must include:

- (i) a survey of the extent, scale and nature of contamination (including ground gases where appropriate);
- (ii) an assessment of the potential risks to:
  - human health,
  - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
  - adjoining land,

- groundwaters and surface waters,
  - ecological systems,
  - archaeological sites and ancient monuments;
- (iii) an appraisal of remedial options, and proposal of the preferred option(s).

This shall be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11'.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

24 Prior to development, a detailed remediation scheme to bring the site to a condition suitable for the intended use (by removing unacceptable risks to human health, buildings and other property and the natural and historical environment) shall be submitted to and approved in writing of the Local Planning Authority. The scheme shall include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme shall ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

25 Prior to first occupation or use, the approved remediation scheme shall be carried out in accordance with its terms and a verification report that demonstrates the effectiveness of the remediation carried out must be produced and is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems.

26 In the event that contamination is found at any time when carrying out the approved development that was not previously identified, it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken and where remediation is necessary a remediation scheme must be prepared, which is subject to the approval in writing of the Local Planning Authority. Following completion of measures identified in the approved

remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

27 A programme of archaeological building recording, specifically a written description and photographic recording of the hospital building to Historic England Level of Recording 3 is required for this application. The archaeological scheme comprises 3 stages of work. Each stage shall be completed and agreed by the Local Planning Authority before it can be approved.

A) No demolition shall take place until a written scheme of investigation (WSI) for building recording has been submitted to and approved by the local planning authority in writing. The WSI should conform to standards set by CYC and the Chartered Institute for Archaeologists.

B) The programme of recording shall be completed in accordance with the programme set out in the Written Scheme of Investigation approved under condition (A) and the provision made for dissemination of results and digital archive deposition with ADS to be secured. This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI.

C) A copy of a report shall be deposited with City of York Historic Environment Record and selection of digital archive images with ADS to allow public dissemination of results within 3 months of completion or such other period as may be agreed in writing with the Local Planning Authority.

This condition is imposed in accordance with Section 16 of NPPF.

Reason: The buildings on this site are of architectural and historic interest and must be recorded prior to demolition.

## **8.0 INFORMATIVES:**

### **Notes to Applicant**

#### **1. STATEMENT OF THE COUNCIL'S POSITIVE AND PROACTIVE APPROACH**

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application.



The Local Planning Authority took the following steps in order to achieve a positive outcome:

Required further details in relation to impacts on trees and highways layout.

## 2. CEMP information:

For noise details on hours of construction, deliveries, types of machinery to be used, use of quieter/silenced machinery, use of acoustic barriers, prefabrication off site etc, should be detailed within the CEMP. Where particularly noisy activities are expected to take place then details should be provided on how they intend to lessen the impact i.e. by limiting especially noisy events to no more than 2 hours in duration. Details of any monitoring may also be required, in certain situation, including the location of positions, recording of results and identification of mitigation measures required.

For vibration details should be provided on any activities which may results in excessive vibration, e.g. piling, and details of monitoring to be carried out. Locations of monitoring positions should also be provided along with details of standards used for determining the acceptability of any vibration undertaken. In the event that excess vibration occurs then details should be provided on how the developer will deal with this, i.e. substitution of driven pile foundations with auger pile foundations. Ideally all monitoring results should be recorded and include what was found and mitigation measures employed (if any).

With respect to dust mitigation, measures may include, but would not be restricted to, on site wheel washing, restrictions on use of unmade roads, agreement on the routes to be used by construction traffic, restriction of stockpile size (also covering or spraying them to reduce possible dust), targeting sweeping of roads, minimisation of evaporative emissions and prompt clean up of liquid spills, prohibition of intentional on-site fires and avoidance of accidental ones, control of construction equipment emissions and proactive monitoring of dust. Further information on suitable measures can be found in the dust guidance note produced by the Institute of Air Quality Management, see <http://iaqm.co.uk/guidance/>. The CEMP must include a site specific risk assessment of dust impacts in line with the IAQM guidance note and include mitigation commensurate with the scale of the risks identified.

For lighting details should be provided on artificial lighting to be provided on site, along with details of measures which will be used to minimise impact, such as restrictions in hours of operation, location and angling of lighting.

In addition to the above the CEMP should provide a complaints procedure, so that in the event of any complaint from a member of the public about noise, dust, vibration or lighting the site manager has a clear understanding of how to respond to complaints received. The procedure should detail how a contact number will be

advertised to the public, what will happen once a complaint had been received (i.e. investigation), any monitoring to be carried out, how they intend to update the complainant, and what will happen in the event that the complaint is not resolved. Written records of any complaints received and actions taken should be kept and details forwarded to the Local Authority every month during construction works by email to the following addresses [public.protection@york.gov.uk](mailto:public.protection@york.gov.uk) and [planning.enforcement@york.gov.uk](mailto:planning.enforcement@york.gov.uk).

### 3. Kitchen odours information:

It is recommended that the applicant refers to the updated Guidance produced by EMAQ in September 2018 titled "Control of Odour and Noise from Commercial Kitchen Exhaust Systems (September 2018)" for further advice on how to comply with this condition. The applicant shall provide information on the location and level of the proposed extraction discharge, the proximity of receptors, size of kitchen or number of covers, and the types of food proposed. A risk assessment in accordance with APPENDIX 3 of the EMAQ guidance shall then be undertaken to determine the level of odour control required. Details should then be provided on the location and size/capacity of any proposed methods of odour control, such as filters, electrostatic precipitation, carbon filters, ultraviolet light/ozon treatment, or odour neutraliser, and include details on the predicted air flow rates in m<sup>3</sup>/s throughout the extraction system.

4. The applicant is reminded that, under the Wildlife and Countryside Act 1981, as amended (section 1), it is an offence to remove, damage or destroy the nest of any wild bird while that nest is in use or being built. Planning consent for a development does not provide a defence against prosecution under this act. Suitable habitat is likely to contain nesting birds between 1st March and 31st August inclusive. As such habitat is present on the application site and is to be assumed to contain nesting birds between the above dates, unless a recent survey has been undertaken by a competent ecologist to assess the nesting bird activity on site during this period and has shown it is absolutely certain that nesting birds are not present.

### 5. EVCP information:

Electric Vehicle Charging Points should incorporate a suitably rated 32A 'IEC 62196' electrical socket to allow 'Mode 3' charging of an electric vehicle. The exact specification is subject to agreement in writing with the council.

Charging points should be located in a prominent position on the site and should be for the exclusive use of zero emission vehicles. Parking bay marking and signage should reflect this.

All electrical circuits/installations shall comply with the electrical requirements in force at the time of installation

**Contact details:**

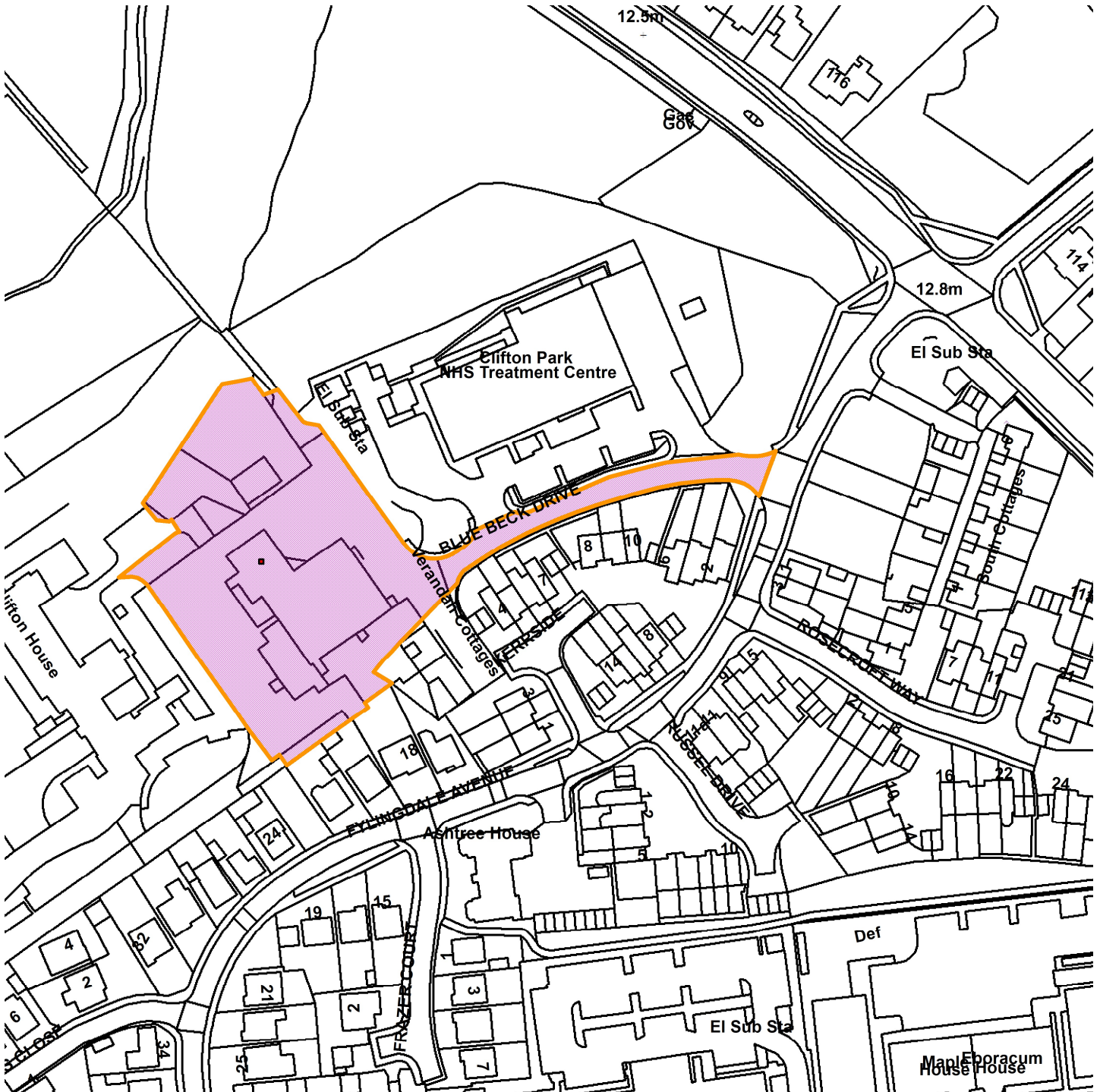
**Case Officer:** Alison Stockdale

**Tel No:** 01904 555730

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22/00707/FULM

York Wheelchair Centre, Bluebeck House, Bluebeck Drive, York



Scale : 1:1616

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<b>Organisation</b>	City of York Council
<b>Department</b>	Directorate of Place
<b>Comments</b>	Site Location Plan
<b>Date</b>	22 July 2022
<b>SLA Number</b>	

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# Planning Committee A

To be held on Thursday 4<sup>th</sup> August 2022 at 4:30pm

# 22/00707/FULM - York Wheelchair Centre, Bluebeck House, Bluebeck Drive, York

Erection of 72no. bedroom care home with associated landscaping following demolition of Blue Beck House and outbuildings



Existing Building - Front



Page 221



Existing Building - Rear

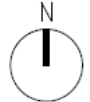


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# Proposed Site Layout



**NOTES**  
 All dimensions are in millimetres unless stated otherwise.  
 Drawings are not to be scaled for Construction purposes.  
 This drawing is to be read in conjunction with all other relevant drawings and specifications.  
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- KEY**
- Application Boundary
  - - - 1.8m high railings with matching gates
  - - - 1.8m high close boarded fence with matching gates
  - Existing boundary wall
  - - - Proposed brick wall to match retained existing one

Rev	Date	Description	Drawn By
P2	16/07/22	Revised to match drawing 0103	RSW
P1	22/05/22	For planning	MLG

**HARRIS IRWIN**  
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 w: www.harrisirwin.com

**Project:**  
 Proposed Care Home  
 Blue Beck House,  
 Shipton Road,  
 York, YO30 5RA  
 client: Torsion Care Limited

**Drawing Title:**  
 Proposed Site Plan - Boundary treatment

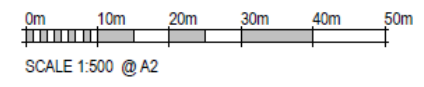
**For Planning Submission**

HIA Project Number	Scale @ A2	Subsidiary				
3165	1:500	00				
Project	Originator	Volume	Level	Type	Role	Number
3165 - HIA	- ZZ	- XX	- DR	- A	-	0107



**0107 Site Plan**  
 1 : 500

City of York Council Planning Committee A Meeting - 4th August 2022



# Proposed Elevations – North and West



**NOTES**  
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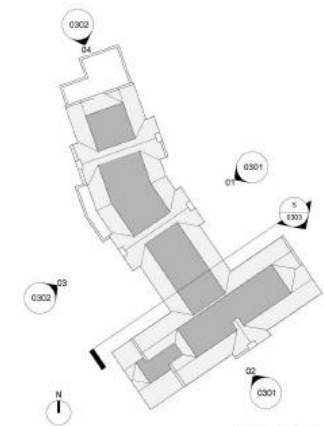
03 Proposed West Elevation  
 1 : 100

### Key to Materials

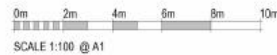
- Buff Brick
- Cast Stone Details
- Fibre Cement Slates
- Grey uPVC windows and doors
- uPVC fascias and soffits
- Black uPVC rainwater goods



04 Proposed North Elevation  
 1 : 100



3 Key Plan  
 1 : 500



No	Date	Description	Drawn	Checked
01	15/05/22	Second Draft Issue	M.S.G.	P.S.W.
02	23/07/22	General changes to elevations	M.S.G.	P.S.W.
03	03/08/22	For Planning Submission	M.S.G.	P.S.W.

**HARRIS IRWIN**  
 architects

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**Project:**  
 Proposed Care Home  
 Blue Beck House,  
 Shipton Road,  
 York, YO30 5RA

**Client:** Torsion Care Limited

**Drawing Title:**  
 Proposed Elevations - North and West

**For Review and Comment**

Rev	Issue	By	Date
01	Issue for Review	M.S.G.	03/08/22



# Proposed Elevations - East and South



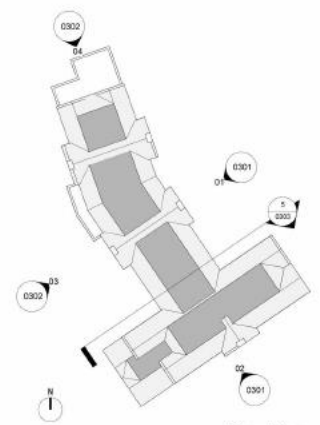
01 Proposed East Elevation  
1 : 100

### Key to Materials

- Buff Brick
- Cast Stone Details
- Fibre Cement Slates
- Grey uPVC windows and doors
- uPVC fascias and soffits
- Black uPVC rainwater goods



02 Proposed South Elevation  
1 : 100



3 Key Plan  
1 : 500

**NOTES**  
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No.	Date	Description	Drawn	Checked	By
01	20/03/22	Contract changes to structure	MLG	BSW	
02	02/04/22	Final Planning Submission	MLG	BSW	
03	11/04/22	Second Draft Issues	MLG	BSW	
04	20/04/22	Final Construction	MLG	BSW	

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**Client:** Torsion Care Limited

**Drawing Title:**  
Proposed Elevations - East and South

**For Review and Comment**

HA Project Number	Drawn By	Checked By	Scale
3385	MLG	BSW	1:50

Project: Proposed Care Home  
3166 - 01 - 20 - 01 - A - 0201

# Proposed Ground Floor Plan



## Room Type Key

- Bedrooms
- Circulation
- Day Rooms
- En-Suites
- Sanitary
- Staff
- Storage

## Summary of Accommodation GIA

Level	Area	Beds	Area per Bed
00 Ground Floor	1326.3 m <sup>2</sup>	22	60.4 m <sup>2</sup>
01 First Floor	1313.1 m <sup>2</sup>	27	48.6 m <sup>2</sup>
02 Second Floor	1146.9 m <sup>2</sup>	23	49.9 m <sup>2</sup>
<b>GIA Total</b>	<b>3786.2 m<sup>2</sup></b>	<b>72</b>	<b>52.6 m<sup>2</sup></b>

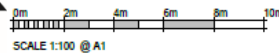
## Day Space

Name	Area	Beds	Area per Bed
<b>00 Ground Floor</b>			
Garden Rooms	33.4 m <sup>2</sup>	22	1.5 m <sup>2</sup>
Clothing	56.4 m <sup>2</sup>	22	2.5 m <sup>2</sup>
Meal Room	17.2 m <sup>2</sup>	22	0.8 m <sup>2</sup>
Meal Lounge	81.3 m <sup>2</sup>	22	3.7 m <sup>2</sup>
Cafe Bar	53.2 m <sup>2</sup>	22	2.4 m <sup>2</sup>
Foyer / Waiting	28.6 m <sup>2</sup>	22	1.3 m <sup>2</sup>
<b>01 First Floor</b>			
Garden Cafe	71.4 m <sup>2</sup>	27	2.6 m <sup>2</sup>
Family Rooms	18.3 m <sup>2</sup>	27	0.7 m <sup>2</sup>
Garden Rooms	33.4 m <sup>2</sup>	27	1.2 m <sup>2</sup>
Clothing	56.4 m <sup>2</sup>	27	2.1 m <sup>2</sup>
Terrace	28.8 m <sup>2</sup>	27	1.1 m <sup>2</sup>
Meal Lounge	49.8 m <sup>2</sup>	27	1.8 m <sup>2</sup>
<b>02 Second Floor</b>			
Activity Lounge	28.8 m <sup>2</sup>	23	1.3 m <sup>2</sup>
Garden Rooms	41.9 m <sup>2</sup>	23	1.8 m <sup>2</sup>
Activity Rooms	49.4 m <sup>2</sup>	23	2.1 m <sup>2</sup>
Terrace	81.6 m <sup>2</sup>	23	3.5 m <sup>2</sup>
Clothing	56.4 m <sup>2</sup>	23	2.4 m <sup>2</sup>
Meal Lounge	49.2 m <sup>2</sup>	23	2.1 m <sup>2</sup>
Terrace	28.3 m <sup>2</sup>	23	1.2 m <sup>2</sup>
<b>Day Space Total (Including Terraces)</b>	<b>886.1 m<sup>2</sup></b>	<b>72</b>	<b>12.3 m<sup>2</sup></b>
<b>Day Space Total (Excluding Terraces)</b>	<b>726.0 m<sup>2</sup></b>	<b>72</b>	<b>10.1 m<sup>2</sup></b>

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0201 Ground Floor  
 1:100



City of York Council Planning Committee A Meeting - 4th August 2022

Rev	Date	Description	By
01	20/05/22	Minor changes to schedule area	MBL
02	10/06/22	For Planning Submission	MBL
03	10/06/22	Second Draft Revise	MBL
04	20/06/22	Schedule finalised	MBL
05	20/06/22	For Comments	MBL
06	20/06/22	For Comments	MBL

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**Client:** Torsion Care Limited

**Drawing Title:**  
 Ground Floor Plan

**For Planning Submission**

Rev	Project Number	Rev	Date	Author
001	3785 - HA - 01 - 00 - DR - A - 0201	01	11/08/22	MBL

# Proposed First Floor Plan

## Room Type Key

- Bedrooms
- Circulation
- Day Rooms
- En-Suites
- Sanitary
- Staff
- Storage

## Summary of Accommodation

### GIA

Level	Area	Beds	Area per Bed
00 Ground Floor	1329.3 m <sup>2</sup>	22	60.4 m <sup>2</sup>
01 First Floor	1313.1 m <sup>2</sup>	27	48.6 m <sup>2</sup>
02 Second Floor	1148.9 m <sup>2</sup>	23	49.9 m <sup>2</sup>
<b>GIA Total</b>	<b>3791.3 m<sup>2</sup></b>	<b>72</b>	<b>52.8 m<sup>2</sup></b>

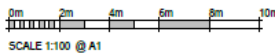
### Day Space

Name	Area	Beds	Area per Bed
<b>00 Ground Floor</b>			
Garden Floor	33.4 m <sup>2</sup>	22	1.5 m <sup>2</sup>
Dining	95.4 m <sup>2</sup>	22	4.3 m <sup>2</sup>
Hot Bistros	112.2 m <sup>2</sup>	22	5.1 m <sup>2</sup>
Main Lounge	81.3 m <sup>2</sup>	22	3.7 m <sup>2</sup>
Cafe Bistros	53.2 m <sup>2</sup>	22	2.4 m <sup>2</sup>
Foyer/Waiting	28.8 m <sup>2</sup>	22	1.3 m <sup>2</sup>
<b>01 First Floor</b>			
Gallery/Cafe	71.4 m <sup>2</sup>	27	2.6 m <sup>2</sup>
Family Room	18.3 m <sup>2</sup>	27	0.7 m <sup>2</sup>
Garden Floor	33.4 m <sup>2</sup>	27	1.2 m <sup>2</sup>
Dining	65.4 m <sup>2</sup>	27	2.4 m <sup>2</sup>
Terrace	28.8 m <sup>2</sup>	27	1.1 m <sup>2</sup>
Main Lounge	48.8 m <sup>2</sup>	27	1.8 m <sup>2</sup>
<b>02 Second Floor</b>			
Sports Lounge	36.8 m <sup>2</sup>	23	1.6 m <sup>2</sup>
Garden Floor	41.9 m <sup>2</sup>	23	1.8 m <sup>2</sup>
Activity Room	48.4 m <sup>2</sup>	23	2.1 m <sup>2</sup>
Terrace	81.3 m <sup>2</sup>	23	3.5 m <sup>2</sup>
Dining	65.4 m <sup>2</sup>	23	2.8 m <sup>2</sup>
Main Lounge	48.2 m <sup>2</sup>	23	2.1 m <sup>2</sup>
Terrace	28.3 m <sup>2</sup>	23	1.2 m <sup>2</sup>
<b>Day Space Total (Including Terraces)</b>	<b>866.1 m<sup>2</sup></b>	<b>72</b>	<b>12.0 m<sup>2</sup></b>
<b>Day Space Total (Excluding Terraces)</b>	<b>728.0 m<sup>2</sup></b>	<b>72</b>	<b>10.1 m<sup>2</sup></b>

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0211 First Floor  
 1 : 100



City of York Council Planning Committee A Meeting - 4th August 2022

No.	Date	Description	By
16	20/03/22	Minor changes to window area	MS, J, SWW
17	20/03/22	For Planning Submission	MS, J, SWW
18	11/05/22	Revised Draft Plans	MS, J, SWW
19	20/05/22	Schedule Approved	SWW, MS, J
20	20/05/22	For Comments	MS, J, SWW

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 York, YO30 5RA

**Client:** Torsion Care Limited

**Drawing Title:**  
 First Floor Plan

**For Planning Submission**

No. of Copies	Date	Issued To	Initials
100	11/05/22	100	MS

3155 - MJA - 01 - 01 - DR - A - 0211

# Proposed Second Floor Plan

## Room Type Key

- Bedrooms
- Circulation
- Day Rooms
- En-Suites
- Sanitary
- Staff
- Storage

## Summary of Accommodation

### GIA

Level	Area	Beds	Area per Bed
00 Ground Floor	1329.3 m <sup>2</sup>	22	60.4 m <sup>2</sup>
01 First Floor	1313.1 m <sup>2</sup>	27	48.6 m <sup>2</sup>
02 Second Floor	1146.9 m <sup>2</sup>	23	49.9 m <sup>2</sup>
<b>GIA Total</b>	<b>3789.3 m<sup>2</sup></b>	<b>72</b>	<b>52.8 m<sup>2</sup></b>

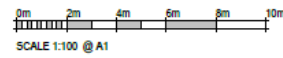
### Day Space

Name	Area	Beds	Area per Bed
<b>00 Ground Floor</b>			
Garden Room	53.4 m <sup>2</sup>	22	2.4 m <sup>2</sup>
Dining	56.4 m <sup>2</sup>	22	2.6 m <sup>2</sup>
Cafe/Bistro	12.2 m <sup>2</sup>	22	0.6 m <sup>2</sup>
Main Lounge	81.5 m <sup>2</sup>	22	3.7 m <sup>2</sup>
Cafe/Bistro	53.2 m <sup>2</sup>	22	2.4 m <sup>2</sup>
Foyer/Waiting	28.8 m <sup>2</sup>	22	1.3 m <sup>2</sup>
<b>01 First Floor</b>			
Day Room	71.4 m <sup>2</sup>	27	2.6 m <sup>2</sup>
Family Room	18.3 m <sup>2</sup>	27	0.7 m <sup>2</sup>
Garden Room	53.4 m <sup>2</sup>	27	2.0 m <sup>2</sup>
Dining	56.4 m <sup>2</sup>	27	2.1 m <sup>2</sup>
Terrace	28.8 m <sup>2</sup>	27	1.1 m <sup>2</sup>
Main Lounge	48.8 m <sup>2</sup>	27	1.8 m <sup>2</sup>
<b>02 Second Floor</b>			
Sports Lounge	36.8 m <sup>2</sup>	23	1.6 m <sup>2</sup>
Garden Room	41.8 m <sup>2</sup>	23	1.8 m <sup>2</sup>
Activity Room	49.4 m <sup>2</sup>	23	2.1 m <sup>2</sup>
Terrace	81.8 m <sup>2</sup>	23	3.5 m <sup>2</sup>
Dining	56.4 m <sup>2</sup>	23	2.4 m <sup>2</sup>
Main Lounge	48.2 m <sup>2</sup>	23	2.1 m <sup>2</sup>
Terrace	28.3 m <sup>2</sup>	23	1.2 m <sup>2</sup>
<b>Day Space Total (Including Terraces)</b>	<b>866.1 m<sup>2</sup></b>	<b>72</b>	<b>12.0 m<sup>2</sup></b>
<b>Day Space Total (Excluding Terraces)</b>	<b>728.0 m<sup>2</sup></b>	<b>72</b>	<b>10.1 m<sup>2</sup></b>

**NOTES**  
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① 0221 Second Floor  
1 : 100



City of York Council Planning Committee A Meeting - 4th August 2022

No	Date	Description	Drawn By	Checked By
01	12/09/20	Issue changes to working area	MB	MB
02	08/09/21	For Planning Submission	MB	MB
03	11/09/21	Second Draft Issue	MB	MB
04	21/09/21	Schedule Amendment	MB	MB
05	02/10/21	For Construction	MB	MB

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Client: Torson Care Limited

Drawing Title:  
Second Floor Plan

For Planning Submission

No	Author	Scale	Date	Issue
01	MB	1:100	02	02

3105 - HA - 01 - 02 - DR - A - 0221



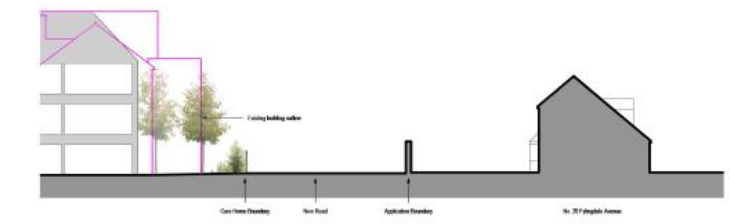
# Proposed Sections including outline of existing



1 Section 1  
1 : 200



2 Section 2  
1 : 200

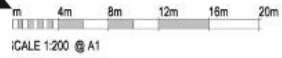


3 Section 3  
1 : 200



4 Key to Sections

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Rev	Date	Description	Drawn	Checked	By
PA	15/07/22	Revised to match drawing DSD	SSW	SSW	
PS	11/04/22	Outline of Existing added	SSW	SSW	
PS	03/03/22	For Planning Submission	SSW	SSW	
PS	11/08/21	Second Draft Issue	SSW	SSW	

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**Client:** Torsion Care Limited

**Drawing Title:**  
 Site Sections

**For Planning Submission**

Warrant Number	Scale @ A1	Validity
1/2022	As indicated	20
3165 - HA - ZZ - XX - CR - A -		0106

# Landscaping Plan



**Do not include drawing (printed or electronic version)**  
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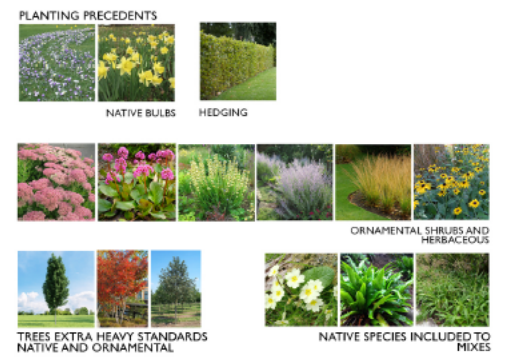


NATIVE SCRUB MIX BASE ROOT (1M CENTRES)				
NORTH EAST PLANTING SPECIES GUIDE				
Quantity	ID	Latin Name	Form	Size
7	G26	<i>Salix repens</i>	1.2m	20cm
6	G26	<i>Salix repens</i>	1.2m	20cm
6	G26	<i>Salix repens</i>	1.2m	20cm

ILLUSTRATIVE NATIVE HEDGE MIX - BASE ROOT (5m WITH CANES AND RAMS)				
Quantity	ID	Latin Name	Scheduled	Height
33	T14	<i>Salix repens</i>	1.1	40-60
33	T14	<i>Salix repens</i>	1.1	40-60
33	T14	<i>Salix repens</i>	1.1	40-60
49	T14	<i>Salix repens</i>	1.1	40-60
49	T14	<i>Salix repens</i>	1.1	40-60
24	T14	<i>Salix repens</i>	1.1	40-60
24	T14	<i>Salix repens</i>	1.1	40-60
33	T14	<i>Salix repens</i>	1.1	40-60
33	T14	<i>Salix repens</i>	1.1	40-60

SEMI-NATURAL / SPECIES / AVENUE TREES (ROOT BALLS)				
Quantity	ID	Latin Name	Grwth or Weight	Form / size
4	Ave 01	<i>Quercus robur</i>	1.4m	10-15m
1	Ave 02	<i>Quercus robur</i>	1.4m	10-15m
4	Ave 03	<i>Quercus robur</i>	1.4m	10-15m
1	Ave 04	<i>Quercus robur</i>	1.4m	10-15m
3	Ave 05	<i>Quercus robur</i>	1.4m	10-15m
1	Ave 06	<i>Quercus robur</i>	1.4m	10-15m
1	Ave 07	<i>Quercus robur</i>	1.4m	10-15m
1	Ave 08	<i>Quercus robur</i>	1.4m	10-15m
1	Ave 09	<i>Quercus robur</i>	1.4m	10-15m
1	Ave 10	<i>Quercus robur</i>	1.4m	10-15m

- KEY**
- PROPOSED TREES
  - NATIVE TREE GROUP 1.5M CENTRES
  - SHRUB AND HERBACEOUS / GROUND COVER PLANTING GROUPS (0.1 - 1.5M)
  - NATIVE HEDGE SINGLE SPECIES (VIEW AT 4M)
  - NATIVE HEDGES 5 OR MORE SPECIES AT 5M
  - NATIVE BULB PLANTING (5M<sup>2</sup>)
  - NATIVE SCRUB PLANTING 1M CENTRES
  - WILD FLOWER 3M<sup>2</sup> WILD FLOWER TURF LTD 200 GRASS / BIRD FLOWERS LAID TO MANUFACTURE SPECIFICATION
  - ROLIWAH AMENITY TURF AND SEEDED EMORGATE EG2 FOR GRASSCRETE



LA-01 Landscape General Arrangement Scale: 1:250



City of York Council Planning Committee A Meeting - 4th August 2022

REFER TO ARCHITECTS AND ENGINEERS EXTERNAL WORKS DRAWINGS FOR FINAL LEVELS, RETAINING WALL CONSTRUCTION, DRAINAGE AND HARD LANDSCAPE DETAILS

REFER TO ARCHITECTS DRAWING FOR FINAL BOUNDARY DETAILS

LAYOUT BASED UPON ARCHITECTS DRAWING 3165-HIA-ZZ-X-X-016A-0103-Proposed Site Plan-CDP-8.dwg

Inclusion of a crossing point to the car park entrance. Location of the staff entrance shown on the plan. Inclusion of an enclosed cycle store to the rear near the staff entrance - 8 spaces	am	11.07.22	C
Adjusted plan to include more trees within groundcover shrub beds to minimise perimeter fence damage	am	08.06.22	B
Adjusted to latest plan 3165-HIA-ZZ-XX-01-0103-Proposed Site Plan-CDP-8.dwg	By	11.03.22	A
	By	Date	Rev
EM	CHKD	SIT	DATE
EM			27.10.21
DWG NO	COUNT	CLIENT	REV
R3-509-03-LA-01		TORSION CARE	C
TITLE	SOFT LANDSCAPE GENERAL ARRANGEMENT PLAN BLUE BECK HOUSE YORK		SCALE
			1:250 A1



Visualisation – Rear



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